

JONES & BEACH ENGINEERS INC.

85 Portsmouth Avenue, PO Box 219, Stratham, NH 03885
603.772.4746 - JonesandBeach.com

September 9, 2022

Rochester Planning Board
Attn. Nel Sylvian
31 Wakefield Street
Rochester, NH 03867



RE: Response Letter
19 Old Gonic Road, Rochester, NH
Tax Map 131, Lot 1
JBE Project No. 21090

Dear Mr. Sylvian,

We are in receipt of comments from Renee Bourdeau, P.E. and Emma Williamson EIT of Geosyntec dated March 10, 2022 and received on March 24, 2022. We had answered as many of the comments as we could with the previous submittal and are updating some of the responses here..

NARRATIVE STORMWATER MANAGEMENT REPORT:

1. *New Comment: The SWPPP should be provided to the city prior to the preconstruction meeting. No further response from the Applicant is required.*
2. *New Comment: The SWPPP should be provided to the city prior to the preconstruction meeting.*
RESPONSE: The SWPPP will be provided prior to the pre-con meeting.
3. *New Comment: No further response is needed from the applicant.*
4. *New comment: No further response is needed from the applicant*
5. *New comment: No further response is needed from the applicant*
6. *New comment: § 218-8.B(1)(g)(i)) requires a description of the procedures to limit and/or optimize use of deicing materials and minimize off-site increase in chloride levels in adjacent surface and groundwater, regardless if it is in a chloride impaired area. This information should be included in the Narrative Stormwater Management and Erosion Control Report.*
RESPONSE: The use of deicing materials will be no different here than from any City owned street. This site will be private, but not treated differently due to safety

of the residents and insurance reasons. We do have stormwater treatment downstream of the road network.

7. *New comment: The SWPPP should be provided to the city prior to the preconstruction meeting*

RESPONSE: The SWPPP is will be provided prior to the pre-con meeting.

8. *New comment: Copies of pertinent state permits should be provided to the city prior to the preconstruction meeting*

RESPONSE: Copies of the State Permits will be provided once received.

SITE DEVELOPMENT PLAN REQUIREMENTS:

1. *New comment: No further response is needed from the applicant*
2. *New comment: No further response is needed from the applicant*
3. *New comment: No further response is needed from the applicant*
4. *New comment: No further response is needed from the applicant*
5. *New comment: No further response from the applicant is needed regarding the locations of earth stockpiles, perimeters controls, construction sites entrances, stone check dams, and other erosion controls. The temporary sediment basin locations should be provided in the SWPPP and provided to the city prior to the preconstruction meeting.*

RESPONSE: Temporary sediment basins have been added to the Phasing Plan.

6. *New comment: The SWPPP should be provided to the city prior to the preconstruction meeting.*

RESPONSE: The SWPPP will be provided prior to the pre-con meeting.

7. *New comment: The SWPPP should be provided to the city prior to the preconstruction meeting.*

RESPONSE: The SWPPP will be provided prior to the pre-con meeting.

8. *New comment: The SWPPP should be provided to the city prior to the preconstruction meeting.*

RESPONSE: The SWPPP will be provided prior to the pre-con meeting.

TEMPORARY CONSTRUCTION STORMWATER DESIGN STANDARDS:

1. *New comment: No further response is needed from applicant.*
2. *New comment: The SWPPP should be provided to the city prior to the preconstruction meeting.*

RESPONSE: The SWPPP will be provided prior to the pre-con meeting.

POST-CONSTRUCTION STORMWATER MANAGEMENT STANDARDS:

1. *New comment: The Applicant has provided a waiver request from § 218-10 C(2), which are the groundwater recharge requirements. The Applicant has not requested a waiver from the site runoff volume requirements in § 218-10 C(3). If the Applicant is requesting a waiver from this section, the waiver request should be amended. Additionally, as stated in § 218-10 C(3)(c), "if an increase in post-development peak rate or volume is anticipated due to site constraints that limit the ability to implement LID measures, the applicant shall demonstrate that the project will not cause adverse impacts to downstream properties, infrastructure, aquatic habitat or water quality degradation in downstream water bodies."*

The Applicant currently makes a statement in only the waiver request as to the site constraints that limit the ability to meet the requirement. However, the Applicant has not demonstrated with supporting calculations to that the project will not cause adverse impacts to downstream properties, infrastructure, aquatic habitat or water quality degradation in downstream water bodies.

The Applicant should provide the calculations and supporting information before the Planning Board approves a Waiver Request

RESPONSE: We have now completed the Drainage Analysis and are submitting the analysis along with this letter. We have also resubmitted the Drainage analysis to NHDES for the Alteration on Terrain (AoT) permit, where we had also requested and received a waiver from NHDES from their Groundwater Recharge requirement.

We have provided a Hydrology and Hydraulics Study to meet 218-10C(2) (b) and to demonstrate that the anticipated increase in stormwater volume coupled with the decrease in peak flow will not cause adverse downstream impacts.

2. *New comment: No further response is needed from applicant.*
3. *New comment: The SWPPP should be provided to the city prior to the preconstruction meeting.*

RESPONSE: The SWPPP will be provided prior to the pre-con meeting.

STORMWATER MANAGEMENT DESIGN STANDARDS FOR NEW DEVELOPMENT:

1. *New comment: The Applicant must demonstrate that the BMPs selected will achieve 50% removal of both total phosphorus and total nitrogen. The Applicant selected wet ponds. Wet ponds are only capable of removing 40% nitrogen load if designed treat 2-inches of runoff (Figure 3-18, Appendix F of MS4 Permit). The Applicant provided calculations for 0.90 inches of runoff which is sufficient for total phosphorus load. The Applicant should select a range of BMPs that are capable of achieving a 50% reduction in nitrogen*

RESPONSE: We have revised all the ponds on the site to be gravel wetlands in order to achieve higher removal rates of phosphorus and total nitrogen. The revised calculations are included in the attached Drainage Analysis.

2. *New comment: The Applicant has not provided a summary table indicating the total required Water Quality Volume for the site and how the proposed practices meet the proposed total volume. The Applicant should also distinguish between Analysis Points. The Applicant should provide a summary table in the Drainage Report to allow the city to easily review this information*

RESPONSE: NH DES AoT BMP worksheets that calculate the required Water Quality Volumes and show how the practices meet the standards are provided within the Drainage Report.

3. *New comment: The Applicant has provided a waiver request from this section and indicates "we are unable to reduce the volume of stormwater to meet the level in pre-existing conditions". The Applicant has not included calculations demonstrating what the required Groundwater Recharge Volume is for the soil type on site and how much recharge to groundwater the Applicant is providing (if any). Further, in accordance with § 218-10.C(2)(b), if infiltration potential is limited or not practical, the Applicant must demonstrate that the stormwater volume from the site will not cause adverse impacts to downstream properties, infrastructure, aquatic habitat, or water quality degradation in downstream water bodies. The Applicant currently makes a statement that there would not be adverse impacts; however, there are no supporting calculations to demonstrate how this conclusion was made.*

RESPONSE: See above response.

4. *New comment: See response to comment IV.1 above. The Applicant has provided a waiver request from § 218-10 C(2), which are the groundwater recharge requirements. The Applicant has not requested a waiver from the site runoff volume requirements in § 218-10 C(3). If the Applicant is requesting a waiver from this section, the waiver request should be amended.*

Additionally, as stated in § 218-10 C(3)(c), "if an increase in post-development peak rate or volume is anticipated due to site constraints that limit the ability to implement LID measures, the applicant shall demonstrate that the project will not cause adverse impacts to downstream properties, infrastructure, aquatic habitat or water quality degradation in downstream water bodies."

The Applicant currently makes a statement in only the waiver request as to the site constraints that limit the ability to meet the requirement. However, the Applicant has not demonstrated with supporting calculations to that the project will not cause adverse impacts to downstream properties, infrastructure, aquatic habitat or water quality degradation in downstream water bodies.

The Applicant should provide the calculations and supporting information before the Planning Board approves a Waiver Request.

RESPONSE: We have completed the Drainage Analysis and are submitting the analysis along with this letter. We have also resubmitted the Drainage analysis to NHDES for the Alteration on Terrain (AoT) permit.

We have provided a Hydrology and Hydraulics Study to meet 218-10C(3)(c) and to demonstrate that the anticipated increase in stormwater volume coupled with the decrease in peak flow will not cause adverse downstream impacts

STORMWATER DRAINAGE SYSTEM SPECIFICATIONS:

1. *New comment: No further response is needed from the applicant.*
2. *New comment: Similar to comment IV.1 above, the Applicant has not demonstrated that the volume discharged from the site from the proposed wet ponds during the 25-year storm will not cause adverse off-site impacts.*

RESPONSE: We have provided a Hydrology and Hydraulics Study to meet 218-10C(3)(c) and to demonstrate that the anticipated increase in stormwater volume coupled with the decrease in peak flow will not cause adverse downstream impacts

3. *New comment: Based on the information provided, multiple drainpipes from the catch basins have velocities greater than 2 feet per second*

RESPONSE: We are respectfully requesting a waiver from this requirement.

4. *New comment: The Applicant should summarize the required velocities in a summary table or in a table on the detail sheet. The velocities entering the proposed swales are not provided within the Drainage Report, based on how the swales were modeled. The swales should be modeled as reaches. The maximum velocities within the swales modeled within the subcatchments all exceed 1 foot per second.*

RESPONSE: We are respectfully requesting a waiver from this requirement.

POST- CONSTRUCTION INSPECTION MAINTENANCE AGREEMENT:

1. *The Inspection and Maintenance Plan should include the following:*
 - a. *New comment: The Inspection and Maintenance Plan is applicable to the stormwater management features at the property. Based on the response from the Applicant, can additional information be provided how the rental unit occupants are supposed to know how to maintain the stormwater management features and provide sufficient funding for these features. This does not seem like a long-term Inspection and Maintenance Plan that will be successful. The Owner of the property should hold this responsibility. The Applicant should provide clarification.*

RESPONSE: The owner of the property is listed as the responsible party in the Inspection and Maintenance report.

- b. *New comment: No further response is needed from the applicant.*

- c. *New comment: No further response is needed from the applicant.*

- d. *New comment: The Inspection and Maintenance report should require that documentation be provided (e.g., photos) of any required maintenance. The I&M document should include a map of all BMPs with a numbering system (e.g., CB1, CB2, Wet Pond 1, Wet Pond 2, etc.) for clarity.*

RESPONSE: This has been addressed in the amended Inspection and Maintenance report.

e. *New comment: No further response is needed from the applicant.*

f. *New comment: The sample deicing log was not included in the I&M document*

RESPONSE: This has been addressed in the amended Inspection and Maintenance report.

g. *New comment: No further response is needed from the applicant.*

h. *New comment: No further response is needed from the applicant.*

OTHER COMMENTS:

1. *New comment: No further response is needed from the applicant.*

2. *New comment: As currently designed the wet ponds have a 4-foot permanent pool. Wet ponds are typically considered unsuitable for residential areas due to the safety risk when they do not have a fence surrounding them. Based on review of the plans, it does not appear that any fencing is proposed around the ponds. A 10-foot safety bench is recommended around wet ponds. Currently the design does not incorporate a safety bench on the wet ponds.*

RESPONSE: We have revised all the ponds on this site to be gravel wetland, which will not have the permanent pool associated with wet ponds. We prefer to not fence these ponds.

3. *No further response is needed from the applicant.*

4. *New comment: No further response is needed from the applicant.*

5. *New comment: No further response is needed from the applicant.*

6. *New comment: The hood is shown on the Catch Basin Detail drawing; however, the treatment rates provided by the catch basins are not included in the Drainage Report.*

RESPONSE: This has been addressed in the amended Drainage Analysis.

7. *New comment: No further response is needed from the applicant.*

8. *New comment: No further response is needed from the applicant.*

9. *New comment: No further response is needed from the applicant.*

10. *New comment: No further response is needed from the applicant.*

11. *The following comments are specific to the HydroCAD Report and Watershed Plans:*

a. *New comment: No further response is needed from the applicant.*

- b. *New comment: The Applicant has not provided a summary table for the volume for each of the storm events. This information should be summarized in the Drainage Report, in the executive summary so the city can easily make this determination.*

RESPONSE: This has been addressed in the amended Drainage Analysis.

- c. *New comment: No further response is needed from the applicant.*

- d. *The height of the vertical grate/orifice opening on the outlet structure for Wet Pond 1 in the HydroCAD model says 10"; however, the detail table shows a change in elevation equal to over 13". The Applicant should reconcile these values.*

RESPONSE: All the ponds along with the associated outlet structures have been revised in the amended Drainage Analysis.

- e. *The applicant has requested to provide the following information in the SWPPP prior to the preconstruction meeting as an alternative to providing the information in the application and response letter. The Planning Board should include this information as a condition if the application is approved.*

- i. *Anticipated project start and completion dates, and duration of grading and construction activities.*
- ii. *Documentation on what LID site planning and design strategies were used on-site or why using LID strategies are not feasible, as required under § 218-8.B(1)(d)*
- iii. *Description of the procedures to control waste, such as discarded building materials, construction debris, sanitary waste, concrete washout, chemicals, and litter.*
- iv. *Locations of temporary sediment basins*
- v. *Locations of equipment storage and staging areas and control procedures*
- vi. *Locations of vehicles fueling areas or equipment fueling areas and control procedures*
- vii. *Location of disposal facilities for solid waste, construction debris, sanitary waste, concrete washout, and plan for stump disposal and control procedures.*
- viii. *Location and description of proposed deicing material storage areas.*

RESPONSE: Agreed.

Included with this response letter are the following:

1. Three (3) Full Size Plan Sets.
2. Sixteen (16) 11x17 Plan Sets (Folded).
3. Amended Drainage Analysis.
4. Stormwater Management Operation and Maintenance Manual.
5. Hydrology and Hydraulics Study by Waterstone Engineering.

Thank you very much for your time.

Very truly yours,
JONES & BEACH ENGINEERS, INC.

Joseph Coronati

Joseph Coronati
Vice President

cc: Green & Company (via email)
John O'Neil (via email)
Derek Caldwell, PE (via email)
Renee Bourdeau, PE (via email)
Dana Webber, Rochester DPW (via email)

PROPOSED SITE PLAN "BAYBERRY COMMONS"

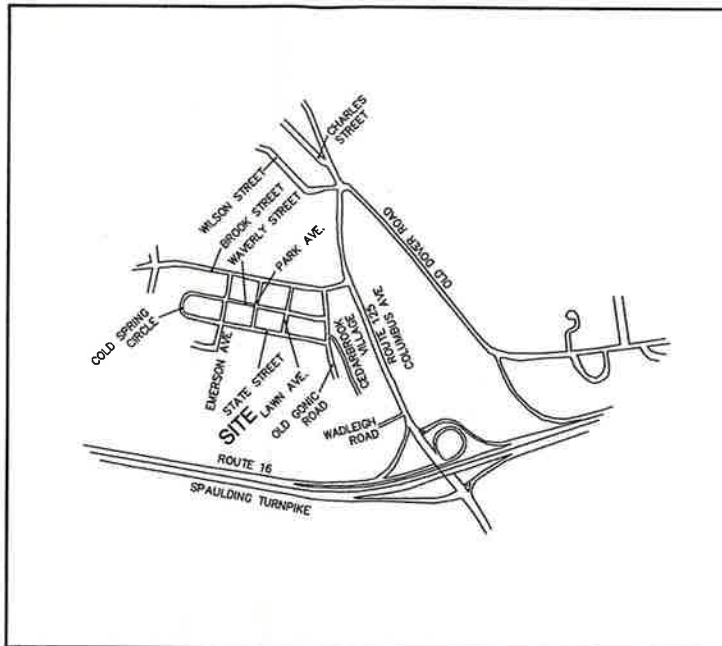
TAX MAP 131, LOT 10
19 OLD GONIC ROAD, ROCHESTER, NH

GENERAL LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROPERTY LINES
---	---	SETBACK LINES
---	---	CENTERLINE
---	---	FRESHWATER WETLANDS LINE
---	---	TIDAL WETLANDS LINE
---	---	STREAM CHANNEL
---	---	TREE LINE
---	---	STONEWALL
---	---	BARBED WIRE
---	---	FENCE
---	---	STOCKADE FENCE
---	---	SOL. BOUNDARY
---	---	AQUIFER PROTECTION LINE
---	---	FLOOD PLAIN LINE
---	---	ZONELINE
---	---	EASEMENT
---	---	MAJOR CONTOUR
---	---	MINOR CONTOUR
---	---	EDGE OF PAVEMENT
---	---	VERTICAL GRANITE CURB
---	---	SLOPE GRANITE CURB
---	---	CAPE COD BERM
---	---	POURED CONCRETE CURB
---	---	ORGANIC FILTER BERM
---	---	DRAINAGE LINE
---	---	SEWER LINE
---	---	SEWER FORCE MAIN
---	---	GAS LINE
---	---	WATER LINE
---	---	WATER SERVICE
---	---	OVERHEAD ELECTRIC
---	---	UNDERGROUND ELECTRIC
---	---	GUARDRAIL
---	---	UNDERDRAIN
---	---	FIRE PROTECTION LINE
---	---	THRUST BLOCK
---	---	IRON PIPE/IRON ROD
---	---	DRILL HOLE
---	---	IRON ROD/DRILL HOLE
---	---	STONE/GRANITE BOUND
---	---	SPOT GRADE
---	---	PAVEMENT SPOT GRADE
---	---	CURB SPOT GRADE
---	---	BENCHMARK (TBM)
---	---	DOUBLE POST SIGN
---	---	SINGLE POST SIGN
---	---	WELL
---	---	TEST PIT
---	---	FAILED TEST PIT
---	---	MONITORING WELL
---	---	PERC TEST
---	---	PHOTO LOCATION
---	---	TREES AND BUSHES
---	---	UTILITY POLE
---	---	LIGHT POLES
---	---	DRAIN MANHOLE
---	---	SEWER MANHOLE
---	---	HYDRANT
---	---	WATER GATE
---	---	WATER SHUT OFF
---	---	REDUCER
---	---	SINGLE GRATE CATCH BASIN
---	---	DOUBLE GRATE CATCH BASIN
---	---	TRANSFORMER
---	---	CULVERT W/MINGWALLS
---	---	CULVERT W/FLARED END SECTION
---	---	CULVERT W/STRAIGHT HEADWALL
---	---	STONE CHECK DAM
---	---	DRAINAGE FLOW DIRECTION
---	---	4K SEPTIC AREA
---	---	WETLAND IMPACT
---	---	VEGETATED FILTER STRIP
---	---	RIPRAP
---	---	OPEN WATER
---	---	FRESHWATER WETLANDS
---	---	TIDAL WETLANDS
---	---	STABILIZED CONSTRUCTION ENTRANCE
---	---	CONCRETE
---	---	GRAVEL
---	---	SNOW STORAGE
---	---	RETAINING WALL

WILDLIFE PROTECTION NOTES:

- ALL OBSERVATIONS OF THREATENED OR ENDANGERED SPECIES SHALL BE REPORTED IMMEDIATELY TO THE NEW HAMPSHIRE FISH AND GAME DEPARTMENT NONGAME AND ENDANGERED WILDLIFE ENVIRONMENTAL REVIEW PROGRAM BY PHONE AT 603-271-2461 AND BY EMAIL AT NHGREVIEW@WILDLIFE.NH.GOV. EMAIL SUBJECT LINE: 10/22/21-3368, BAYBERRY COMMONS, WILDLIFE SPECIES OBSERVATION.
- PHOTOGRAPHS OF THE OBSERVED SPECIES AND NEARBY ELEMENTS OF HABITAT OR AREAS OF LAND DISTURBANCE SHALL BE PROVIDED TO NHF&G IN DIGITAL FORMAT AT THE ABOVE EMAIL ADDRESS FOR VERIFICATION AS FEASIBLE.
- IN THE EVENT A THREATENED OR ENDANGERED SPECIES IS OBSERVED ON THE PROJECT SITE DURING THE TERM OF THE PERMIT, THE SPECIES SHALL NOT BE DISTURBED, HANDLED, OR HARMED IN ANY WAY PRIOR TO CONSULTATION WITH NHF&G AND IMPLEMENTATION OF CORRECTIVE ACTIONS RECOMMENDED BY NHF&G, IF ANY, TO ASSURE THE PROJECT DOES NOT APPRECIABLY JEOPARDIZE THE CONTINUED EXISTENCE OF THREATENED AND ENDANGERED SPECIES AS DEFINED IN ITS 1002.04.
- THE NHF&G, INCLUDING ITS EMPLOYEES AND AUTHORIZED AGENTS, SHALL HAVE ACCESS TO THE PROPERTY DURING THE TERM OF THE PERMIT.



LOCUS MAP
SCALE 1" = 2000'

CIVIL ENGINEER / SURVEYOR

JONES & BEACH ENGINEERS, INC.
85 PORTSMOUTH AVENUE
PO BOX 219
STRATHAM, NH 03885
(603) 772-4746
CONTACT: JOSEPH CORONATI
EMAIL: JCORONATI@JONESANDBEACH.COM

TRAFFIC ENGINEER

STEPHEN G. PERNAW & COMPANY, INC.
P.O. BOX 1821
CONCORD, NH 03302
(603) 731-8500
CONTACT: STEPHEN G. PERNAW
EMAIL: SGP@PERNAW.COM

WETLAND CONSULTANT

GOVE ENVIRONMENTAL SERVICES, INC.
8 CONTINENTAL DR., BUILDING 2, UNIT H
EXETER, NH 03833-7526
(603) 778-0644
CONTACT: JAMES GOVE
EMAIL: JGOVE@GESINC.BIZ

LANDSCAPE DESIGNER

LM LAND DESIGN, LLC
11 SOUTH ROAD
BRENTWOOD, NH 03833
(603) 770-7728
CONTACT: LISE McNAUGHTON
LMLANDDESIGN@GMAIL.COM

WATER AND SEWER

ROCHESTER DEPARTMENT OF PUBLIC WORKS
45 OLD DOVER ROAD
ROCHESTER, NH 03867
(603) 332-4096
CONTACT: MICHAEL BEZANSON, P.E.

ELECTRIC

EVERSOURCE ENERGY
74 OLD DOVER ROAD
ROCHESTER, NH 03867
(603) 555-5334
CONTACT: MARK BOUCHER

TELEPHONE

CONSOLIDATED COMMUNICATIONS
1575 GREENLAND ROAD
GREENLAND, NH 03840
(603) 427-5525
CONTACT: JOE CONSIDINE

CABLE TV

COMCAST COMMUNICATION CORPORATION
334-B CALEF HIGHWAY
EPPING, NH 03042-2325
(603) 679-5695

NATURAL GAS

UNITIL SERVICE CORP.
325 WEST ROAD
PORTSMOUTH, NH 03801
(603) 294-5261
MACLEAND@UNITIL.COM

SHEET INDEX

CS	COVER SHEET
OVEX	EXISTING CONDITIONS OVERVIEW PLAN
C1-C4	EXISTING CONDITIONS PLAN
DM1	DEMOLITION PLAN
OVRS	OVERVIEW SITE PLAN
C5-C8	SITE PLANS
OVRG	OVERVIEW GRADING PLAN
C9-C11	GRADING AND DRAINAGE PLAN
OVRU	OVERVIEW UTILITY PLAN
U1-U2	UTILITY PLAN
L1-L2	LANDSCAPE PLAN
L3-L4	LIGHTING PLAN
P1-P5	ROAD PLAN AND PROFILE
P6-P10	SEWER PROFILE
OFF-1-5	OFFSITE IMPROVEMENT PLANS
D1-D4	DETAIL SHEETS
E1	EROSION AND SEDIMENT CONTROL DETAILS
PH	PHASING PLAN

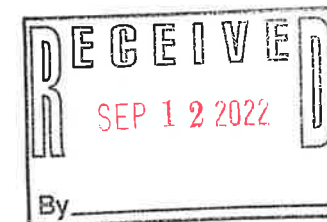
PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

APPROVED - ROCHESTER, NH
PLANNING BOARD

DATE:



Design: JAC	Draft: LAZ	Date: 04/29/21
Checked: JAC	Scale: AS NOTED	Project No.: 21090
Drawing Name: 21090-PLAN.dwg		
THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM JONES & BEACH ENGINEERS, INC. (JBE). ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO JBE.		



REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/23/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
Designed and Produced in NH
85 Portsmouth Ave. PO Box 219 Stratham, NH 03885
Civil Engineering Services
603-772-4746
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

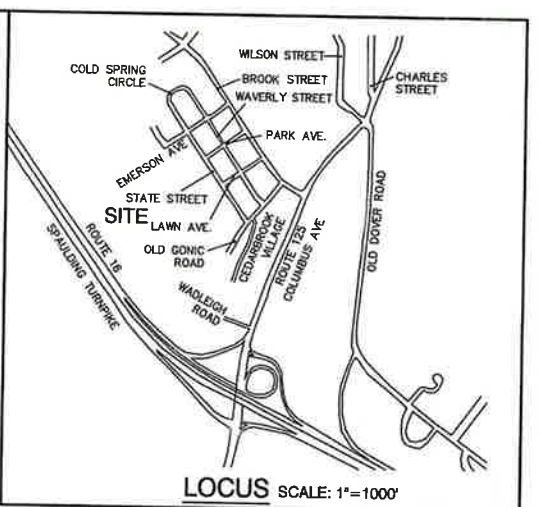
Plan Name:	COVER SHEET
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.	CS
SHEET 1 OF 46	JBE PROJECT NO. 21090

PROJECT NAME AND LOCATION
JBE # 21090 REVISION 0, 11/22/21



- EXISTING CONDITIONS NOTES:**
- UNDERGROUND FACILITIES, UTILITIES AND STRUCTURES HAVE BEEN PLOTTED FROM FIELD OBSERVATION AND THEIR LOCATION MUST BE CONSIDERED APPROXIMATE ONLY. NEITHER JONES & BEACH ENGINEERS, INC. NOR ANY OF THEIR EMPLOYEES TAKE RESPONSIBILITY FOR THE LOCATION OF ANY UNDERGROUND STRUCTURES OR UTILITIES NOT SHOWN. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE ALL UNDERGROUND STRUCTURES AND/OR UTILITIES LOCATED PRIOR TO EXCAVATION WORK BY CALLING 1-888-800-SAFE (1-888-800-7233).
 - VERTICAL DATUM: NAVD 88. HORIZONTAL DATUM: STATE PLANE COORDINATES.
 - THE SUBJECT PARCEL IS PARTIALLY LOCATED WITHIN AN AREA HAVING A ZONE AE DESIGNATION BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), ON FLOOD INSURANCE RATE MAP NO. 330170221D, WITH EFFECTIVE DATE OF MAY 17, 2005 FOR COMMUNITY PANEL NO. 211 OF 405, IN ROCKINGHAM COUNTY, STATE OF NEW HAMPSHIRE, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR COMMUNITY IN WHICH SAID PREMISES IS SITUATED.
 - THE LIMITS OF JURISDICTIONAL WETLANDS WERE DELINEATED BY JAMES GOVE DURING SPRING, 2021 IN ACCORDANCE WITH THE FOLLOWING GUIDANCE DOCUMENTS:
 - THE CORPS OF ENGINEERS FEDERAL MANUAL FOR IDENTIFYING AND DELINEATING JURISDICTIONAL WETLANDS.
 - THE NORTH CENTRAL & NORTHEAST REGIONAL SUPPLEMENT TO THE FEDERAL MANUAL.
 - THE CURRENT VERSION OF THE FIELD INDICATORS FOR IDENTIFYING HYDRIC SOILS IN NEW ENGLAND, AS PUBLISHED BY THE NEW ENGLAND INTERSTATE WATER POLLUTION CONTROL COMMISSION AND FOR THE CURRENT VERSION OF THE FIELD INDICATORS OF HYDRIC SOILS IN THE UNITED STATES, AS PUBLISHED BY THE USDA, NRCS, AS APPROPRIATE.
 - THE CURRENT NATIONAL LIST OF PLANT SPECIES THAT OCCUR IN WETLANDS, AS PUBLISHED BY THE US FISH AND WILDLIFE SERVICE.
 - WETLAND BOUNDARIES AND CONSTRUCTION LIMITS ARE TO BE CLEARLY MARKED PRIOR TO THE START OF CONSTRUCTION.



ABUTTERS:

131/08-1 LAURA GATCHELL 1A CEDARBROOK AVE ROCHESTER, NH 03867 4653/126 (5/5/19)	131/08-17 ALICIA MANSON 3C CEDARBROOK AVE ROCHESTER, NH 03867 4623/411 (11/27/16)	131/08-33 DEBRA WELCH 6A CEDARBROOK AVE ROCHESTER, NH 03867 4645/275 (4/3/10)	131/08-49 MARISSA CORBIN 8E CEDARBROOK AVE ROCHESTER, NH 03867 4645/275 (4/3/10)	131/08-65 LYNNE PARADIS 11C CEDARBROOK AVE ROCHESTER, NH 03867 3120/280 (12/21/04)
131/08-2 KATHRYN SOUSA 3D CEDARBROOK AVE ROCHESTER, NH 03867 2985/618 (3/30/2004)	131/08-18 EVANS FAMILY TRUST 3D CEDARBROOK AVE ROCHESTER, NH 03867 4757/589 (4/9/20)	131/08-34 JOSEPH ZUROWSKIS 6B CEDARBROOK AVE ROCHESTER, NH 03867 2110/732 (5/26/1999)	131/08-50 JUSTIN MANTEUFFEL 8F CEDARBROOK AVE ROCHESTER, NH 03867 3522/214 (4/2/07)	131/08-66 GRANT REALTY TRUST ATTN: DAVID PAOLINI 242 CENTRAL AVE DOVER, NH 03820 4178/531 (11/5/13)
131/08-3 ANNA & ROBERT PEREIRA 1C CEDARBROOK AVE ROCHESTER, NH 03867 4546/289 (1/31/16)	131/08-19 JESSICA STENIER 3E CEDARBROOK AVE ROCHESTER, NH 03867 4487/437 (6/22/17)	131/08-35 ONCOWARSTO MULIAWATI 6C CEDARBROOK AVE ROCHESTER, NH 03867 4312/136 (7/24/15)	131/08-51 JEFFREY BOOMER 9A CEDARBROOK AVE ROCHESTER, NH 03867 4576/729 (6/17/16)	131/08-67 USA KIMBALL 11E CEDARBROOK AVE ROCHESTER, NH 03867 4589/936 (8/2/18)
131/08-4 AMY SCHAEFFER 1D CEDARBROOK AVE ROCHESTER, NH 03867 4387/484 (5/26/16)	131/08-20 SEAN CONNORS 3F CEDARBROOK AVE ROCHESTER, NH 03867 4598/273 (8/27/16)	131/08-36 JOSHUA SWONGER 6D CEDARBROOK AVE ROCHESTER, NH 03867 4956/1035 (9/21/21)	131/08-52 KENNETH MAUSER 9B CEDARBROOK AVE ROCHESTER, NH 03867 3935/182 (6/26/11)	131/08-68 CHARLENE WHITEHOUSE 11F CEDARBROOK AVE ROCHESTER, NH 03867 2417/55 (11/25/01)
131/08-5 MICHAEL PRIOR 1E CEDARBROOK AVE ROCHESTER, NH 03867 4655/795 (5/19/19)	131/08-21 SPRASH GAMBRONE 4A CEDARBROOK AVE ROCHESTER, NH 03867 3018/408 (6/20/04)	131/08-37 LINDSEY WICKLESS 6E CEDARBROOK AVE ROCHESTER, NH 03867 4814/926 (10/1/20)	131/08-53 HOMER WOODBURY 9C CEDARBROOK AVE ROCHESTER, NH 03867 3165/820 (3/31/05)	131/08-69 CHRISTINE SENECHAL 12A CEDARBROOK AVE ROCHESTER, NH 03867 4428/130 (10/25/16)
131/08-6 DAVID PETTIS 1F CEDARBROOK AVE ROCHESTER, NH 03867 4658/411 (5/30/19)	131/08-22 BRIAN MCQUADE 274 CEDAR ST SEBASTIAN, FL 32058 3911/711 (1/13/11)	131/08-38 LOUISE BOUCHER 6F CEDARBROOK AVE ROCHESTER, NH 03867 1977/283 (1/11/1998)	131/08-54 JAMES SAULNIER 9D CEDARBROOK AVE ROCHESTER, NH 03867 4579/933 (6/28/16)	131/08-70 STEPHEN SMALL 12 B CEDARBROOK AVE ROCHESTER, NH 03867 2543/142 (7/11/02)
131/08-7 AMANDA LAMBERT 1C CEDARBROOK AVE ROCHESTER, NH 03867 3631/316 (3/27/08)	131/08-23 KERRY DESAUTEL 4C CEDARBROOK AVE ROCHESTER, NH 03867 3935/932 (6/27/11)	131/08-39 ERIN FERLAND 7D CEDARBROOK AVE ROCHESTER, NH 03867 4983/870 (10/5/21)	131/08-55 ALISON JESSEMAN 9E CEDARBROOK AVE ROCHESTER, NH 03867 4579/933 (6/28/16)	131/08-71 SARAH BENTON 12C CEDARBROOK AVE ROCHESTER, NH 03867 4573/310 (8/3/18)
131/08-8 CAROLINE LEWIS 1H CEDARBROOK AVE ROCHESTER, NH 03867 4597/717 (8/30/18)	131/08-24 PATRICK RILEY 4D CEDARBROOK AVE ROCHESTER, NH 03867 2400/678 (10/30/01)	131/08-40 ROBIN GARY 7B CEDARBROOK AVE ROCHESTER, NH 03867 4541/860 (1/4/18)	131/08-56 CHEN YIRU 9F CEDARBROOK AVE ROCHESTER, NH 03867 4580/875 (8/14/19)	131/08-72 ROBERT THOMAS 12D CEDARBROOK AVE ROCHESTER, NH 03867 4680/875 (8/14/19)
131/08-9 STEPHEN & PATRICIA TALOR 2A CEDARBROOK AVE ROCHESTER, NH 03867 2392/538 (10/15/01)	131/08-25 PAULIE PRATT 4E CEDARBROOK AVE ROCHESTER, NH 03867 2916/48 (12/11/03)	131/08-41 BRANDON MAINS 7C CEDARBROOK AVE ROCHESTER, NH 03867 4629/242 (1/10/19)	131/08-57 UNWAMBYCH ESPERANCE 10A CEDARBROOK AVE ROCHESTER, NH 03867 4960/191 (9/23/21)	131/08-73 SUSAN WELSH 12E CEDARBROOK AVE ROCHESTER, NH 03867 3032/39 (7/2/04)
131/08-10 BRIAN HERBERT 2B CEDARBROOK AVE ROCHESTER, NH 03867 3567/428 (8/23/07)	131/08-26 TIMUR GAMIROV 4F CEDARBROOK AVE ROCHESTER, NH 03867 4296/903 (5/28/15)	131/08-42 JOA FOREMAN 7D CEDARBROOK AVE ROCHESTER, NH 03867 4598/155 (8/30/18)	131/08-58 DANIEL DEGRANDPRE 10B CEDARBROOK AVE ROCHESTER, NH 03867 4658/850 (5/29/19)	131/08-74 MELINDA RIGGER REV. TRUST 12F CEDARBROOK AVE ROCHESTER, NH 03867 4304/153 (6/25/15)
131/08-11 BUDI TANUDHARTHA 2C CEDARBROOK AVE ROCHESTER, NH 03867 429/512 (5/7/15)	131/08-27 HAYDEN GRACE KRISTY 5A CEDARBROOK AVE ROCHESTER, NH 03867 4684/777 (6/19/19)	131/08-43 MELANIE HOLT 7E CEDARBROOK AVE ROCHESTER, NH 03867 4798/961 (8/17/20)	131/08-59 RALPH STILLINGS 10C CEDARBROOK AVE ROCHESTER, NH 03867 4525/479 (10/26/17)	131/08-75 CITY OF ROCHESTER 31 WAKEFIELD ST ROCHESTER, NH 03867 3371/144 (4/10/06)
131/08-12 GEORGE GATCOMB 2D CEDARBROOK AVE ROCHESTER, NH 03867 3328/679 (1/26/06)	131/08-28 LINDA LANCHANCE 5B CEDARBROOK AVE ROCHESTER, NH 03867 3477/108 (12/21/06)	131/08-44 ALEXANDRA WICKMAN JOSEPH WARREN 7F CEDARBROOK AVE ROCHESTER, NH 03867 4765/858 (5/28/20)	131/08-60 DENISE SELFE 10D CEDARBROOK AVE ROCHESTER, NH 03867 2215/260 (4/16/2000)	131/08-76 MELINDA RIGGER REV. TRUST 12G CEDARBROOK AVE ROCHESTER, NH 03867 4504/153 (6/25/15)
131/08-13 RAYMOND KING 2E CEDARBROOK AVE ROCHESTER, NH 03867 3474/028 (12/18/06)	131/08-29 MICHAEL WALSH 5C CEDARBROOK AVE ROCHESTER, NH 03867 4705/193 (10/30/16)	131/08-45 KAREN BAILEY 8A CEDARBROOK AVE ROCHESTER, NH 03867 3876/380 (10/27/10)	131/08-61 RICHARD DUBSETT 33 ALEXANDRA LANE ROCHESTER, NH 03867 1304/258 (4/15/1987)	131/08-77 CITY OF ROCHESTER 31 WAKEFIELD ST ROCHESTER, NH 03867 3371/144 (4/10/06)
131/08-14 OAK BLUFF REALTY LLC 242 CENTRAL AVE DOVER, NH 03820 3953/549 (9/8/11)	131/08-30 SANDRA FOURNIER 5D CEDARBROOK AVE ROCHESTER, NH 03867 2942/181 (2/11/04)	131/08-46 MICHAEL GROVETTI 8B CEDARBROOK AVE ROCHESTER, NH 03867 4715/120 (12/2/19)	131/08-62 JOHN COLECHIA 10 F CEDARBROOK AVE ROCHESTER, NH 03867 4792/125 (5/7/20)	131/08-78 MICHAEL GROVETTI TRUST 21 RIDGEWOOD ROAD ELIOT, MAINE 03903 4581/977 (6/26/18)
131/08-15 SEAN JACKSON 3A CEDARBROOK AVE ROCHESTER, NH 03867 4581/644 (6/28/18)	131/08-31 OLD DOG PROPERTIES LLC 24 MORRISON LANE DOVER, NH 03820 4027/158 (5/29/12)	131/08-47 PHILIP LANG 8C CEDARBROOK AVE ROCHESTER, NH 03867 4581/977 (6/26/18)	131/08-63 TARA CANFIELD 11A CEDARBROOK AVE ROCHESTER, NH 03867 2443/774 (1/10/02)	131/08-79 MOORE FAMILY REV. TRUST 21 RIDGEWOOD ROAD ELIOT, MAINE 03903 4581/977 (6/26/18)
131/08-16 ELIZABETH HANSON 3B CEDARBROOK AVE ROCHESTER, NH 03867 4457/435 (2/9/17)	131/08-32 ELIZABETH RANKS 235 LONG POND ROAD DANVILLE, NH 03819 3719/769 (3/10/09)	131/08-48 ROBIN LORD 8D CEDARBROOK AVE ROCHESTER, NH 03867 4923/182 (6/22/21)	131/08-64 MOORE FAMILY REV. TRUST 21 RIDGEWOOD ROAD ELIOT, MAINE 03903 4581/977 (6/26/18)	131/08-80 MOORE FAMILY REV. TRUST 21 RIDGEWOOD ROAD ELIOT, MAINE 03903 4581/977 (6/26/18)

PLAN REFERENCES:

- "CEDARBROOK VILLAGE CONDOMINIUM BOUNDARY SURVEY FOR ROUNDTREE REALTY TRUST ROCHESTER, NH", PREPARED BY BERRY CONSTRUCTION CO., INC.; DATED APRIL 23, 1986; RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN P23-059.
- "PLAN OF LAND PARCEL NO. 5 TAX ASSESSOR'S MAP NO. 59 AND PARCEL NO. 2 TAX ASSESSOR'S MAP NO. 59 HAMPSHIRE ROUTE 125 PREPARED FOR JEB-SOOT REALTY, INC.", PREPARED BY CIVILWORKS ENGINEERS AND SURVEYORS; DATED NOVEMBER 6, 1989 RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN P37A-028.
- "PROPOSED SUBDIVISION AND LOT LINE REVISION FOR ROCHESTER HOUSING AUTHORITY COLD SPRING MANOR BROOK STREET ROCHESTER, NH", PREPARED BY BERRY SURVEYING & ENGINEERING; DATED FEBRUARY 13, 2001; RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN P61-056.
- "PROPOSED SUBDIVISION LAND OF THOMAS & JODI REMINGTON 15 OLD GONIC ROAD ROCHESTER, NH", PREPARED BY BERRY SURVEYING & ENGINEERING; DATED AUGUST 17, 2005; RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN P81-0036.
- "SUBDIVISION PLAN TAX MAP 131, LOT 26 WAVERLY ST./ OLD GONIC RD. ROCHESTER NH FOR BAYONE CONSTRUCTION COMPANY, INC.; PREPARED BY NORWAY PLAINS ASSOCIATES, INC.; DATED JULY 25, 2006; RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN P68-0100.
- "SUBDIVISION PLAN TAX MAP 131, LOT 57 STATE STREET & EMERSON COURT ROCHESTER NH FOR NORWAY PLAINS ASSOCIATES, INC.; DATED OCTOBER 17, 2007; RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN P92-014.
- "ROCHESTER HEIGHTS ROCHESTER, NH A.H. CHAPMAN LAND CO"; PREPARED BY E.H. SHEFFIELD; DATED JULY 1920; RECORDED AT THE STRAFFORD COUNTY REGISTRY OF DEEDS AS PLAN #19 POCKET #10 FOLDER #3.

CERTIFICATION:

I CERTIFY THAT THIS PLAN WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN UNADJUSTED LINEAR ERROR OF CLOSURE THAT EXCEEDS BOTH THE MINIMUM OF 1:10,000 AS DEFINED IN SECTION 503.04 OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES AND THE MINIMUM OF 1:15,000 AS DEFINED IN SECTION 4.2 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

THIS SURVEY CONFORMS TO A CATEGORY 1 CONDITION 1 SURVEY AS DEFINED IN SECTION 4.1 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

I CERTIFY THAT THIS SURVEY IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.



DAVID M. COLLIER, LLS 892
ON BEHALF OF JONES & BEACH ENGINEERS, INC.

SOIL NOTES:

THIS MAP PRODUCT IS WITHIN THE TECHNICAL STANDARDS OF THE NATIONAL COOPERATIVE SOIL SURVEY. IT IS A SPECIAL PURPOSE PRODUCT, INTENDED FOR INFILTRATION REQUIREMENTS BY THE NH DES ALTERATION OF TERRAIN BUREAU. IT WAS PRODUCED BY A PROFESSIONAL SOIL SCIENTIST, AND IS NOT A PRODUCT OF THE USDA NATURAL RESOURCES CONSERVATION SERVICE. THERE IS A REPORT THAT ACCOMPANIES THIS MAP.

THE SITE SPECIFIC SOIL SURVEY (SSSS) WAS PRODUCED JULY 21, 2021, AND WAS PREPARED BY JAMES P. GOVE, GSS # 004, GOVE ENVIRONMENTAL SERVICES, INC. THE SURVEY AREA IS LOCATED AT 19 OLD GONIC ROAD, ROCHESTER, NH.

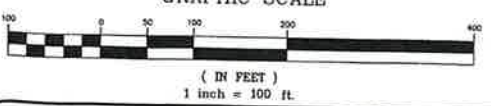
SOILS WERE IDENTIFIED WITH THE NEW HAMPSHIRE STATE-WIDE NUMERICAL SOILS LEGEND, USDA NRCS, DURIAM, NH. ISSUE # 10, JANUARY 2011. THE NUMERIC LEGEND WAS AMENDED TO IDENTIFY THE CORRECT SOIL COMPONENTS OF THE COMPLEX.

HYDROLOGIC SOIL GROUP FROM KSAT VALUES FOR NEW HAMPSHIRE SOILS, SOCIETY OF SOIL SCIENTISTS OF NEW ENGLAND, SPECIAL PUBLICATION NO. 5, SEPTEMBER, 2009.

SSSS SYM.	SSSS MAP NAME	HISS SYM.	HYDRO.	SOIL GRP.
853	BOXFORD (SOMEWHAT POORLY DRAINED)	453	C	
38	ELDRIDGE LOAMY SAND	343	C	
141	HOLLIS-ROCK OUTCROP-CHATFIELD	228	D	
448	SQUATITE FINE SANDY LOAM	323	C	
5	ODDORTHENTS, LOAMY	762	N/A	
538	SQUAMSCOTT LOAMY SAND	543	C	

SLOPE PHASE:
0-8% B 8-15% C 15-25% D
25%+ E

GRAPHIC SCALE



Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21060
Drawing Name: 21060-PLAN.dwg

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM JONES & BEACH ENGINEERS, INC. (JBE). ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO JBE.



REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
Civil Engineering Services
85 Portsmouth Ave. PO Box 219 Stratham, NH 03885
603-772-4746 FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

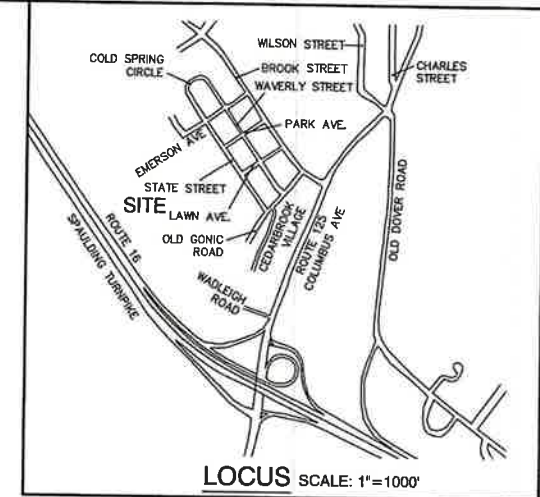
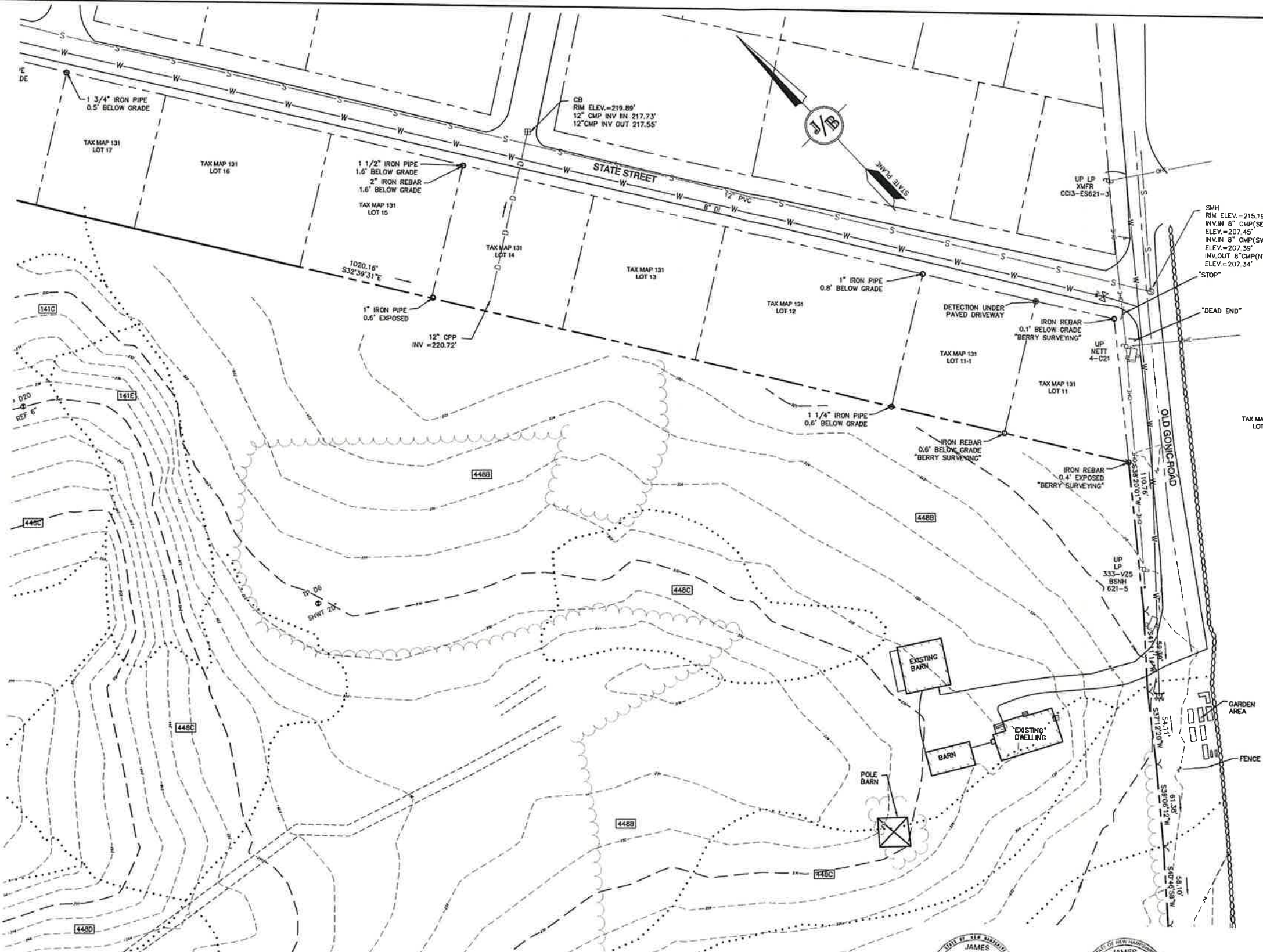
Plan Name: **EXISTING CONDITIONS PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309.695 SQ. FT. ±
30.07 ACRES ±

DRAWING No.
OVR
SHEET 2 OF 46
JBE PROJECT NO. 21060



SOIL NOTES:

THIS MAP PRODUCT IS WITHIN THE TECHNICAL STANDARDS OF THE NATIONAL COOPERATIVE SOIL SURVEY. IT IS A SPECIAL PURPOSE PRODUCT, INTENDED FOR INFILTRATION REQUIREMENTS BY THE NH DES ALTERATION OF TERRAIN BUREAU. IT WAS PRODUCED BY A PROFESSIONAL SOIL SCIENTIST, AND IS NOT A PRODUCT OF THE USDA NATURAL RESOURCES CONSERVATION SERVICE. THERE IS A REPORT THAT ACCOMPANIES THIS MAP.

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SOILS WERE IDENTIFIED WITH THE NEW HAMPSHIRE STATE-WIDE NUMERICAL SOILS LEGEND, USDA NRCS, DURHAM, NH, ISSUE # 10, JANUARY 2011. THE NUMERIC LEGEND WAS AMENDED TO IDENTIFY THE CORRECT SOIL COMPONENTS OF THE COMPLEX.

HYDROLOGIC SOIL GROUP FROM KSAT VALUES FOR NEW HAMPSHIRE SOILS, SOCIETY OF SOIL SCIENTISTS OF NEW ENGLAND, SPECIAL PUBLICATION NO. 5, SEPTEMBER, 2009.

SSSS SYM.	SSSS MAP NAME	HISS SYM.	HYDRO. SOIL GRP.
953	BOXFORD (SOMEWHAT POORLY DRAINED)	453	C
38	ELDRIDGE LOAMY SAND	343	C
141	HOLLIS-ROCK OUTCROP-CHATFIELD	228	D
448	SITUATE FINE SANDY LOAM	323	C
5	ODDORTHEM, LOAMY	762	N/A
538	SQUAMSCOTT LOAMY SAND	543	C

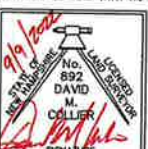
SLOPE PHASE:
 0-8% B 8-15% C 15-25% D
 25%+ E

CERTIFICATION:

I CERTIFY THAT THIS PLAN WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN UNADJUSTED LINEAR ERROR OF CLOSURE THAT EXCEEDS BOTH THE MINIMUM OF 1:10,000 AS DEFINED IN SECTION 503.04 OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES AND THE MINIMUM OF 1:15,000 AS DEFINED IN SECTION 4.2 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

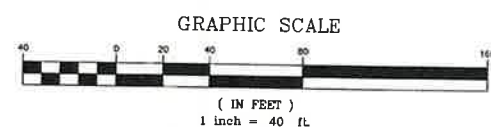
THIS SURVEY CONFORMS TO A CATEGORY 1 CONDITION 1 SURVEY AS DEFINED IN SECTION 4.1 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

I CERTIFY THAT THIS SURVEY PLAN IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.



DAVID M. COLLIER, LLS 892
ON BEHALF OF JONES & BEACH ENGINEERS, INC.

DATE: 9/9/2022



Design: JAC Draft: LAZ Date: 04/29/21
 Checked: JAC Scale: AS NOTED Project No.: 21090
 Drawing Name: 21090-PLAN.dwg

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM JONES & BEACH ENGINEERS, INC. (JBE). ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO JBE.



REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES Aot COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES Aot COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/18/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
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 85 Portsmouth Ave. PO Box 219 Stratham, NH 03885
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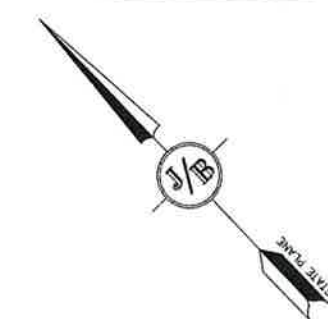
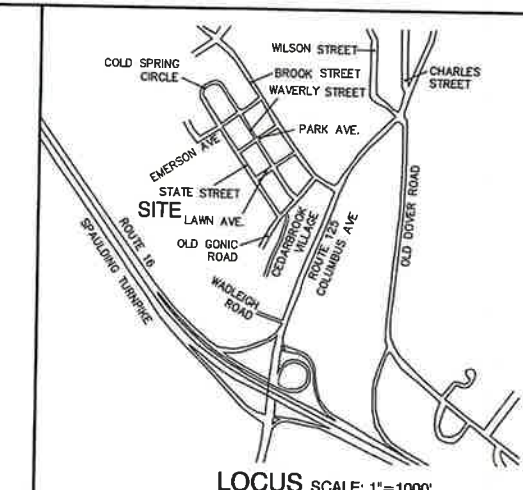
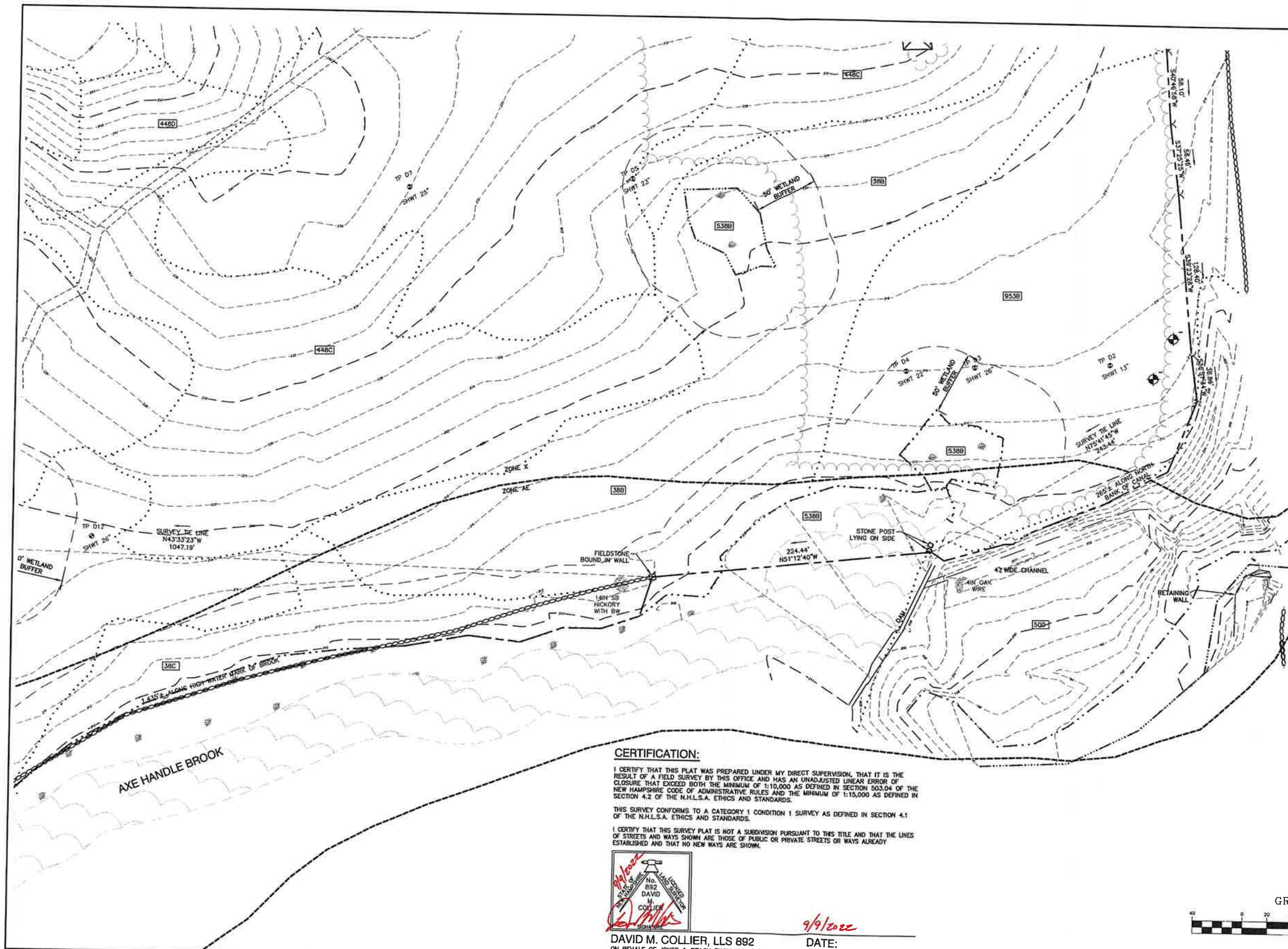
Plan Name: **EXISTING CONDITIONS PLAN**
 Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
 Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,895 SQ. FT. ±
30.07 ACRES ±

DRAWING No.
C1
SHEET 3 OF 46
JBE PROJECT NO. 21090



SOIL NOTES:

THIS MAP PRODUCT IS WITHIN THE TECHNICAL STANDARDS OF THE NATIONAL COOPERATIVE SOIL SURVEY. IT IS A SPECIAL PURPOSE PRODUCT, INTENDED FOR INFILTRATION REQUIREMENTS BY THE NH DES ALTERATION OF TERRAIN BUREAU. IT WAS PRODUCED BY A PROFESSIONAL SOIL SCIENTIST, AND IS NOT A PRODUCT OF THE USDA NATURAL RESOURCES CONSERVATION SERVICE. THERE IS A REPORT THAT ACCOMPANIES THIS MAP. THE SITE SPECIFIC SOIL SURVEY (SSSS) WAS PRODUCED JULY 21, 2021, AND WAS PREPARED BY JAMES P. GOVE, CSS # 004, GOVE ENVIRONMENTAL SERVICES, INC. THE SURVEY AREA IS LOCATED AT 19 OLD GONIC ROAD, ROCHESTER, NH. SOILS WERE IDENTIFIED WITH THE NEW HAMPSHIRE STATE-WIDE NUMERICAL SOILS LEGEND, USDA NRCS, DURHAM, NH, ISSUE # 10, JANUARY 2011. THE NUMERIC LEGEND WAS AMENDED TO IDENTIFY THE CORRECT SOIL COMPONENTS OF THE COMPLEX. HYDROLOGIC SOIL GROUP FROM KSAT VALUES FOR NEW HAMPSHIRE SOILS, SOCIETY OF SOIL SCIENTISTS OF NEW ENGLAND, SPECIAL PUBLICATION NO. 5, SEPTEMBER, 2009.

SSSS SYM.	SSSS MAP NAME	HISS SYM.	HYDRO. SOIL GRP.
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38	ELDRIDGE LOAMY SAND	343	C
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448	SCITUATE FINE SANDY LOAM	323	C
5	DOUBORTHEM, LOAMY	762	N/A
538	SQUAMSCOTT LOAMY SAND	543	C

SLOPE PHASE:
 0-8% B 8-15% C 15-25% D
 25%+ E

CERTIFICATION:

I CERTIFY THAT THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN UNADJUSTED LINEAR ERROR OF CLOSURE THAT EXCEEDS BOTH THE MINIMUM OF 1:10,000 AS DEFINED IN SECTION 503.04 OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES AND THE MINIMUM OF 1:15,000 AS DEFINED IN SECTION 4.2 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

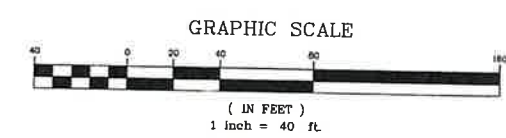
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I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.



DAVID M. COLLIER, LLS 892
 ON BEHALF OF JONES & BEACH ENGINEERS, INC.

DATE: 9/9/2022



PROJECT PARCEL
 CITY OF ROCHESTER
 TAX MAP 131, LOT 10

APPLICANT
 GREEN AND COMPANY
 11 LAFAYETTE ROAD
 NORTH HAMPTON, NH 03862

TOTAL LOT AREA
 1,309,895 SQ. FT. ±
 30.07 ACRES ±

Design: JAC	Draft: LAZ	Date: 04/28/21
Checked: JAC	Scale: AS NOTED	Project No.: 21090
Drawing Name: 21090-PLAN.dwg		

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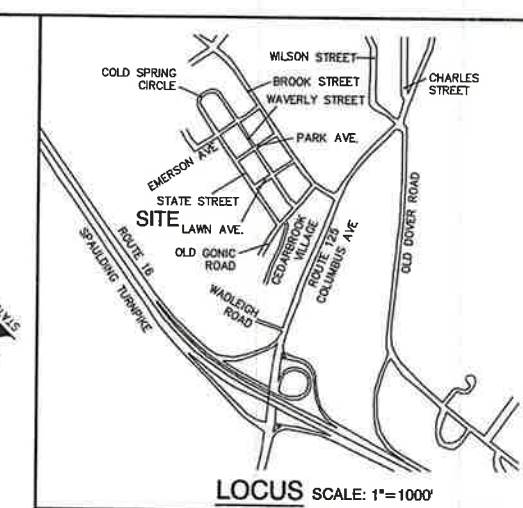
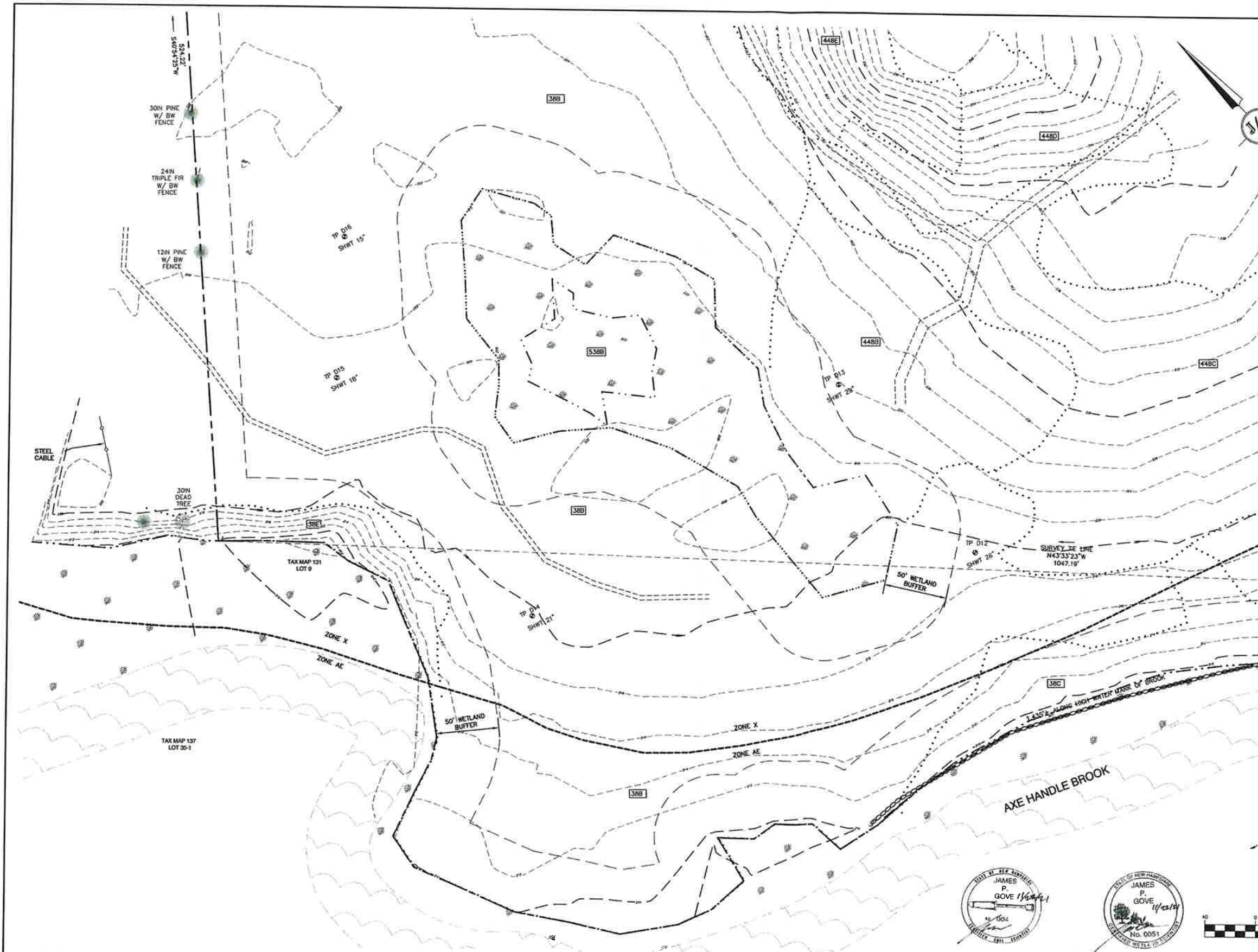


REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	9/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
 Designed and Produced in NH
 85 Portsmouth Ave. PO Box 219 Stratham, NH 03886
 Civil Engineering Services
 603-772-4746 FAX: 603-772-0227
 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	EXISTING CONDITIONS PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
C2
 SHEET 4 OF 48
 JBE PROJECT NO. 21090



SOIL NOTES:

THIS MAP PRODUCT IS WITHIN THE TECHNICAL STANDARDS OF THE NATIONAL COOPERATIVE SOIL SURVEY. IT IS A SPECIAL PURPOSE PRODUCT, INTENDED FOR INFILTRATION REQUIREMENTS BY THE NH DES ALTERATION OF TERRAIN BUREAU. IT WAS PRODUCED BY A PROFESSIONAL SOIL SCIENTIST, AND IS NOT A PRODUCT OF THE USDA NATURAL RESOURCES CONSERVATION SERVICE. THERE IS A REPORT THAT ACCOMPANIES THIS MAP.

THE SITE SPECIFIC SOIL SURVEY (SSSS) WAS PRODUCED JULY 21, 2021, AND WAS PREPARED BY JAMES P. GOVE, CSS # 004, GOVE ENVIRONMENTAL SERVICES, INC. THE SURVEY AREA IS LOCATED AT 19 OLD GONIC ROAD, ROCHESTER, NH.

SOILS WERE IDENTIFIED WITH THE NEW HAMPSHIRE STATE-WIDE NUMERICAL SOILS LEGEND, USDA NRCS, DURHAM, NH, ISSUE # 10, JANUARY 2011. THE NUMERIC LEGEND WAS AMENDED TO IDENTIFY THE CORRECT SOIL COMPONENTS OF THE COMPLEX.

HYDROLOGIC SOIL GROUP FROM KSAT VALUES FOR NEW HAMPSHIRE SOILS, SOCIETY OF SOIL SCIENTISTS OF NEW ENGLAND, SPECIAL PUBLICATION NO. 5, SEPTEMBER, 2009.

SSSS SYM.	SSSS MAP NAME	HISS SYM.	HYDRO.	SOIL GRP.
953	BOXFORD (SOMEWHAT POORLY DRAINED)	453	C	
38	ELDRIDGE LOAMY SAND	343	C	
141	HOLLIS-ROCK OUTCROP-CHATFIELD	228	D	
448	SCITUATE FINE SANDY LOAM	323	C	
5	ODDORIENTS, LOAMY	762	N/A	
538	SQUAMSCOTT LOAMY SAND	543	C	

SLOPE PHASE:
0-8% B 8-15% C 15-25% D
25%+ E

CERTIFICATION:

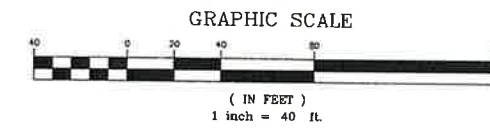
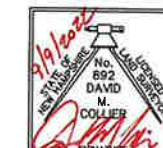
I CERTIFY THAT THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN UNADJUSTED LINEAR ERROR OF CLOSURE THAT EXCEEDS BOTH THE MINIMUM OF 1:10,000 AS DEFINED IN SECTION 503.04 OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES AND THE MINIMUM OF 1:15,000 AS DEFINED IN SECTION 4.2 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

THIS SURVEY CONFORMS TO A CATEGORY 1 CONDITION 1 SURVEY AS DEFINED IN SECTION 4.1 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

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DAVID M. COLLIER, ILS 892
ON BEHALF OF JONES & BEACH ENGINEERS, INC.

DATE: 9/9/2022



Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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J/B Jones & Beach Engineers, Inc.
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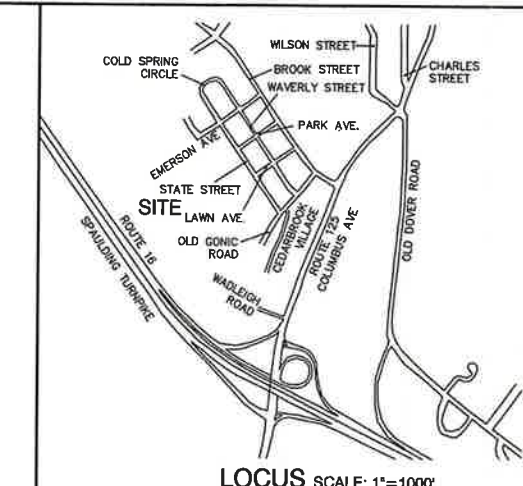
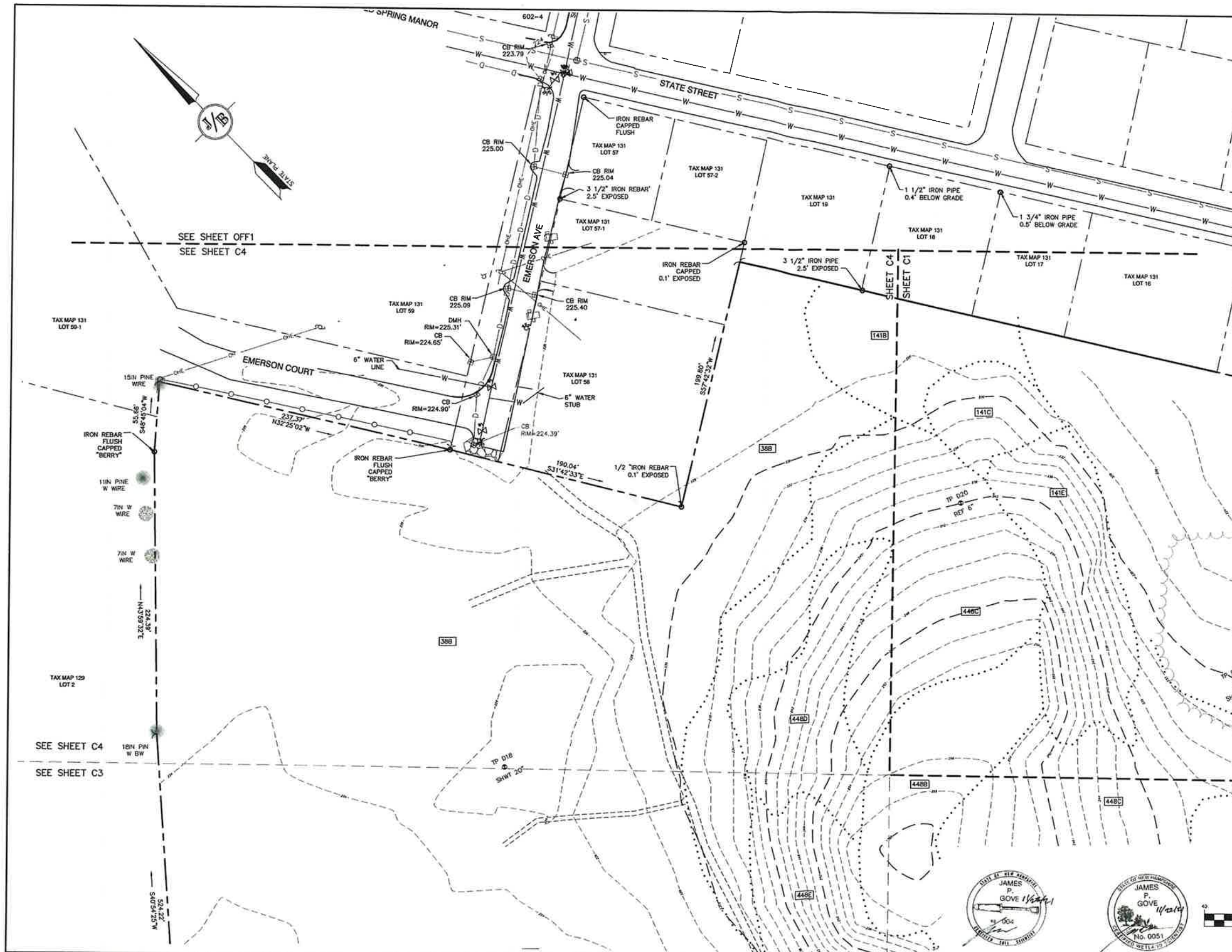
Plan Name: **EXISTING CONDITIONS PLAN**
Project: **BAYBERRY COMMONS
19 OLD GONIC ROAD, ROCHESTER, NH**
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148**

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

DRAWING No.
C3
SHEET 5 OF 46
JBE PROJECT NO. 21090



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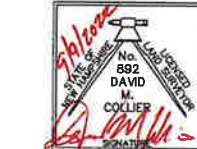
SLOPE PHASE:
0-5% B 8-15% C 15-25% D
25%+ E

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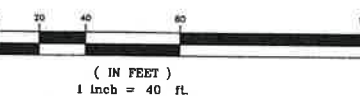
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DAVID M. COLLIER, LLS 892
ON BEHALF OF JONES & BEACH ENGINEERS, INC.

DATE: 9/9/2022

GRAPHIC SCALE



Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg
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Civil Engineering Services
603-772-4746 FAX: 603-772-0227 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	EXISTING CONDITIONS PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

DRAWING No.
C4
SHEET 6 OF 46
JBE PROJECT NO. 21090



DEMOLITION NOTES:

1. THIS PLAN IS INTENDED TO PROVIDE MINIMUM GUIDELINES FOR SITE DEMOLITION. IT SHOULD BE NOTED THAT ALL MANMADE FEATURES, PAVEMENT, SIGNS, POLES, CURBING, CONCRETE WALKS, UTILITIES, ETC., SHALL BE REMOVED AS NECESSARY TO CONSTRUCT WORK, UNLESS OTHERWISE NOTED TO REMAIN. THROUGHOUT THE CONSTRUCTION PROCESS, THE CONTRACTOR SHALL INFORM THE ENGINEER IMMEDIATELY OF ANY FIELD DISCREPANCIES FROM DATA AS SHOWN ON DESIGN PLANS. THIS INCLUDES ANY UNFORESEEN CONDITIONS, SUBSURFACE OR OTHERWISE FOR EVALUATION AND RECOMMENDATIONS. ANY CONTRADICTION BETWEEN ITEMS OF THIS PLAN/PLAN SET, OR BETWEEN THE PLANS AND ON-SITE CONDITIONS MUST BE RESOLVED BEFORE RELATED CONSTRUCTION HAS BEEN INITIATED.
2. A TEMPORARY CULVERT AND ROADBED SHALL BE IN PLACE PRIOR TO ANY USE OF A WETLAND CROSSING.
3. WETLAND IMPACTS SHALL NOT OCCUR UNTIL ALL PERMITS HAVE BEEN ACQUIRED AND IMPACT MITIGATION REQUIREMENTS HAVE BEEN SATISFIED.
4. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR IS REQUIRED TO HAVE THE PROJECT LAND SURVEYOR STAKE OR FLAG CLEARING LIMITS. A MINIMUM OF 48 HOURS NOTICE IS REQUIRED. CLEARING LIMITS ARE THE EDGE OF THE PROPERTY AND THE LIMITS OF WORK.
5. ALL EXISTING STRUCTURES WITHIN THE CONSTRUCTION AREA, UNLESS OTHERWISE NOTED TO REMAIN, SHALL BE REMOVED AND DISPOSED OF OFF-SITE IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL GUIDELINES. ANY BURNING ON-SITE SHALL BE SUBJECT TO LOCAL ORDINANCES.
6. ALL EXISTING PAVED SURFACES WITHIN THE LIMITS OF WORK THAT ARE TO REMAIN SHALL BE RECLAIMED TO MINIMUM DEPTH OF 12" AND REGRADED AS SHOWN ON THE GRADING AND DRAINAGE PLAN. RECLAIMED ASPHALT SHALL CONFORM TO STATE SPECIFICATIONS. PRIOR TO REMOVAL, PAVEMENT SHALL BE SAWCUT AT ALL ENTRANCES AND LIMITS OF REMOVAL.
7. ALL EXISTING GRANITE CURBING TO BE REMOVED SHALL BE STOCKPILED IN AN AREA TO BE DESIGNATED BY THE OWNER OR OWNER'S REPRESENTATIVE. THE OWNER SHALL INSPECT GRANITE CURBING TO BE RESET AND APPROVE LOCATION OF RESET CURBING. THE CONTRACTOR SHALL NOT INSTALL USED CURBING AT ANY ENTRANCE LOCATIONS.
8. ALL EXISTING UTILITIES SHALL BE TERMINATED AT THE PROPERTY LINE, UNLESS OTHERWISE NOTED ON THE PLANS, IN CONFORMANCE WITH LOCAL, STATE AND UTILITY COMPANY STANDARDS. SPECIFICATIONS AND DETAILS, THE CONTRACTOR SHALL COORDINATE UTILITY SERVICE DISCONNECTS WITH THE UTILITY REPRESENTATIVES PRIOR TO THE START OF WORK.
9. EXISTING WATERLINES AND HYDRANTS TO BE REMOVED SHALL BE CAPPED AT EXISTING WATERMAIN.
10. EXISTING GAS SERVICE LINES ARE TO BE REMOVED ON-SITE UP TO EXISTING GASMAIN LINES OR VALVES.
11. THE CONTRACTOR SHALL REMOVE AND PROPERLY DISPOSE OF ALL CONTAMINATED MATERIAL LOCATED IN THE AREA OF EXISTING LEACHFIELDS IN ACCORDANCE WITH LOCAL AND STATE REGULATIONS.
12. ALL CURBING, CONCRETE, PAVEMENT, BUILDINGS AND SUBBASE MATERIALS LOCATED WITHIN PROPOSED LANDSCAPED AREAS SHALL BE REMOVED AND REPLACED WITH LOAM MATERIALS SUITABLE FOR LANDSCAPING IN ACCORDANCE WITH TECHNICAL SPECIFICATIONS. (SEE ALSO LANDSCAPE PLAN).
13. SEE LANDSCAPE PLAN FOR "TREES TO BE SAVED" AND DETAILS ASSOCIATED WITH LANDSCAPED AREAS.
14. THE CONTRACTOR SHALL OBTAIN TREE CLEARING PERMIT FROM LOCAL AND STATE AUTHORITIES PRIOR TO START OF CONSTRUCTION (IF REQUIRED).
15. CONTRACTOR SHALL HAVE THE OPTION TO REMOVE DRAINAGE/SEWER STRUCTURES, OR REMOVE MANHOLE FRAME AND GRATE/COVER TO A MINIMUM OF 36" BELOW FINISH GRADE, FRACTURE BOTTOM AND FILL WITH COMPACTED BORROW.
16. IN AREAS WHERE CONSTRUCTION IS PROPOSED ADJACENT TO ABUTTING PROPERTIES, THE CONTRACTOR SHALL INSTALL ORANGE CONSTRUCTION FENCING ALONG PROPERTY LINES IN ALL AREAS WHERE SILT FENCING IS NOT REQUIRED.
17. EXISTING SANITARY SEWER LINE AND STRUCTURES LOCATED WITHIN THE EXISTING SEWER EASEMENTS SHALL BE MAINTAINED OR MODIFIED AS NOTED ON PLANS. SEWER LINES SHALL BE DISCONNECTED IN ACCORDANCE WITH LOCAL STANDARDS AND REGULATIONS.
18. EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO CONSTRUCTION AND ANY EARTH MOVING OPERATIONS. SILT FENCE SHALL BE INSTALLED AT THE LIMITS OF IMPACT AREAS ACCORDING TO THE DETAILS SHOWN ON SHEET E1.
19. EXCAVATED MATERIALS WILL BE PLACED WITHIN UPLAND AREAS AS FILL MATERIAL OR HAULED OFF-SITE FOR DISPOSAL IN AN APPROPRIATE UPLAND LOCATION.

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,865 SQ. FT. ±
30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg
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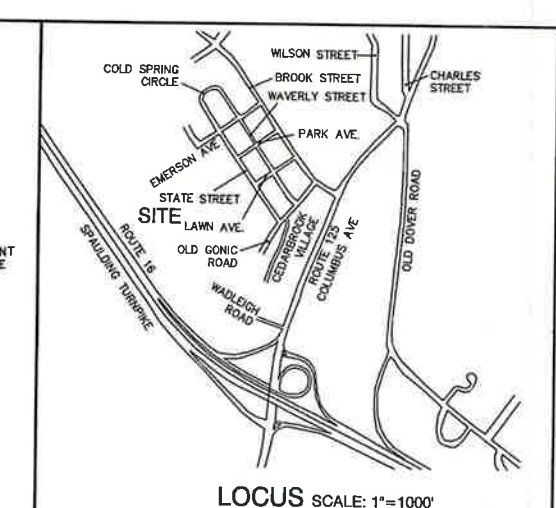
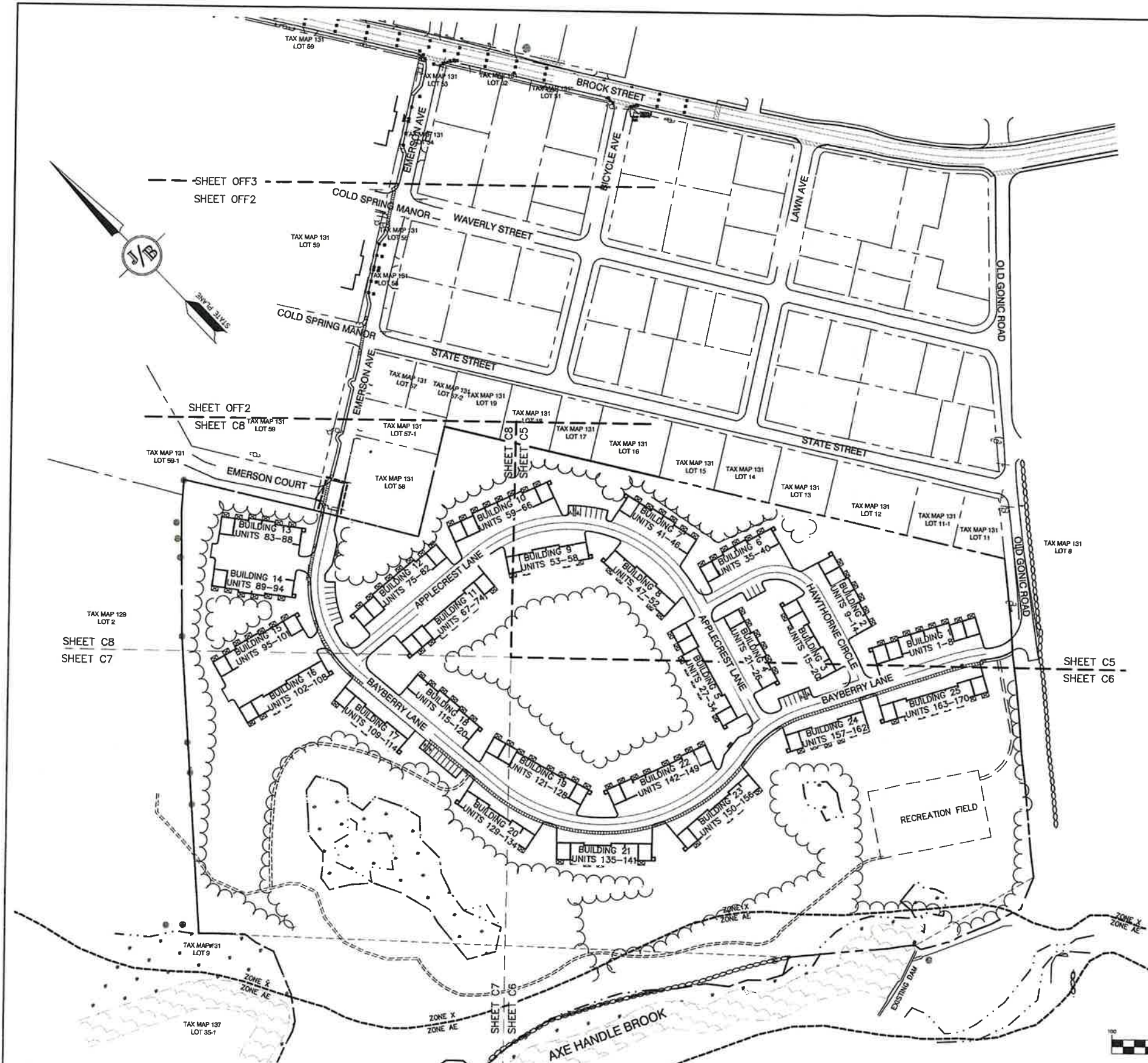


10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
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REV.	DATE	REVISION	BY

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85 Portsmouth Ave. Civil Engineering Services 603-772-4746
PO Box 219 Stratham, NH 03885 FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	DEMOLITION PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

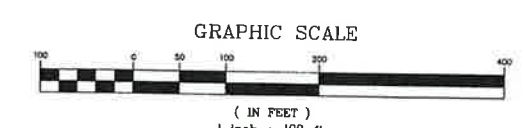
DRAWING No.
DM-1
SHEET 7 OF 48
JBE PROJECT NO. 21090



SITE NOTES:

- THE INTENT OF THIS PLAN IS TO SHOW A 170-UNIT TOWNHOUSE DEVELOPMENT ON THE SUBJECT PARCEL. ALL UNITS TO HAVE 3 BEDROOMS, PROJECT TO BE SERVED BY CITY WATER AND SEWER AND UNDERGROUND UTILITIES.
- ZONING DISTRICT: RESIDENTIAL-2, USE: FIVE-OR MORE FAMILY
LOT AREA MINIMUM = 30,000 SF
LOT FRONTAGE MINIMUM = 100'
BUILDING SETBACKS (MINIMUM):
FRONT SETBACK = 15'
SIDE SETBACK = 10'
REAR SETBACK = 25'
WETLAND SETBACK = 50'
MAX. BUILDING HEIGHT = 35'
MAX. BUILDING FOOTPRINT = 30%
MAX. LOT COVERAGE = 60%
MIN. LOT AREA / DWELLING UNIT = 7,500 S.F.
BUILDING FOOTPRINT PROPOSED = 122,400 S.F. = 9.3% OF SITE
TOTAL LOT COVERAGE PROPOSED = 310,478 S.F. = 23.7% OF SITE
- DENSITY CALCULATION:
TOTAL LOT AREA = 1,309,695 S.F. / 7,500 S.F. = 175 UNITS ALLOWED
170 UNITS PROVIDED
- LOT COVERAGE CALCULATIONS

	AREA (SF)	AREA (AC)	PERCENT OF SITE
TOTAL AREA	1,309,695 SF	30.07 AC	
BUILDING W/ OVERHANG AND DECKS	148,840 SF	3.4 AC	11.2%
PAVEMENT	188,078 SF	4.3 AC	14.4%
TOTAL LOT COVERAGE	344,918 SF	7.7 AC	25.6%
LANDSCAPING OR GRASS	332,984 SF	7.6 AC	25.4%
POND AREA	42,215 SF	1.0 AC	3.2%
TOTAL DISTURBED AREA	710,117 SF	16.3 AC	54.2%
WETLAND	58,294 SF	1.3 AC	4.5%
UNDISTURBED UPLAND	541,284 SF	12.4 AC	41.3%
TOTAL UNDISTURBED AREA	599,578 SF	13.8 AC	45.8%
- PARKING CALCULATIONS
ALL UNITS TO HAVE 2 GARAGE SPACES AND 2 SPACES IN FRONT OF UNIT PLUS
34 VISITOR SPACES INCLUDING 6 HANDICAP SPACES.
TOTAL PARKING ON SITE = 730 SPACES
- ALL CONSTRUCTION SHALL CONFORM TO CITY STANDARDS AND REGULATIONS, AND NHDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, WHICHEVER IS MORE STRINGENT.
- THE SUBJECT PARCEL IS PARTIALLY LOCATED WITHIN AN AREA HAVING A ZONE AE DESIGNATION BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), ON FLOOD INSURANCE RATE MAP NO. 33017C0210, WITH EFFECTIVE DATE OF MAY 17, 2005 FOR COMMUNITY PANEL NO. 211 OF 405, IN ROCKINGHAM COUNTY, STATE OF NEW HAMPSHIRE, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR COMMUNITY IN WHICH SAID PREMISES IS SITUATED.
- LANDOWNERS ARE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL WETLAND REGULATIONS, INCLUDING PERMITTING REQUIRED UNDER THESE REGULATIONS.
- ALL CONSTRUCTION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (S.W.P.P.P.). THIS DOCUMENT IS TO BE KEPT ONSITE AT ALL TIMES AND UPDATED AS REQUIRED.
- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER, ARCHITECT AND/OR OWNER, IN ORDER TO OBTAIN AND/OR PAY ALL THE NECESSARY LOCAL PERMITS, FEES AND BONDS.
- ALL PROPOSED SIGNAGE SHALL CONFORM WITH THE CITY ZONING REGULATIONS, UNLESS A VARIANCE IS OTHERWISE REQUESTED.
- ALL SIGNAGE AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) AND NHDOT STANDARDS AND SPECIFICATIONS (NON-REFLECTORIZED PAVEMENT MARKINGS), UNLESS OTHERWISE NOTED.
- ALL PARKING STALLS SHALL BE SEPARATED USING 4" WIDE SOLID STRIPES. STRIPING SHALL HAVE TWO COATS OF PAINT, ALKYL BASIN SYNTHETIC RESIN, FEDERAL SPECIFICATION TTP-115 TYPE 1, IN A COLOR OF WHITE.
- ALL STOP BARS SHALL BE 18" IN WIDTH IN A COLOR OF WHITE; ALL TRAFFIC ARROWS SHALL BE PAINTED IN A COLOR OF WHITE.
- ALL BUILDING DIMENSIONS SHALL BE VERIFIED WITH THE ARCHITECTURAL AND STRUCTURAL PLANS PROVIDED BY THE OWNER. ANY DISCREPANCIES SHOULD BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND OWNER PRIOR TO THE START OF CONSTRUCTION. BUILDING DIMENSIONS AND AREAS TO BE TO OUTSIDE OF MASONRY, UNLESS OTHERWISE NOTED.
- SNOW TO BE STORED AT EDGE OF PAVEMENT AND IN AREAS SHOWN ON THE PLANS, OR TRUCKED OFFSITE TO AN APPROVED SNOW DUMPING LOCATION.
- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO LABOR OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) RULES AND REGULATIONS.
- TRASH TO BE HANDLED WITH TOTES AT EACH UNIT. TRASH TO BE PICKED UP BY A PRIVATE HAULER.
- STATE PERMITS REQUIRED:
NHDES ALTERATION OF TERRAIN PERMIT
NHDES SEWER EXTENSION PERMIT
NHDES DRINKING WATER PERMIT
- EXCAVATION PERMITS ARE REQUIRED FOR ANY WORK PERFORMED WITHIN THE CITY RIGHT-OF-WAY.



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

APPROVED - ROCHESTER, NH
PLANNING BOARD

DATE:

Design: JAC Draft: LAZ Date: 04/29/21
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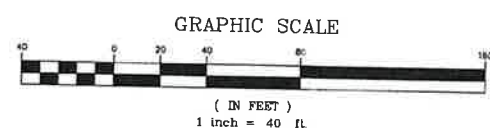
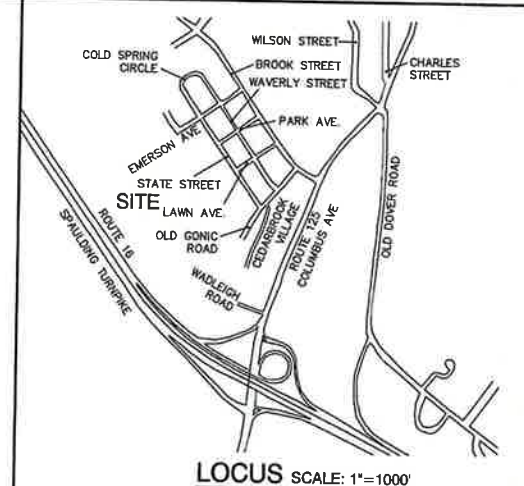
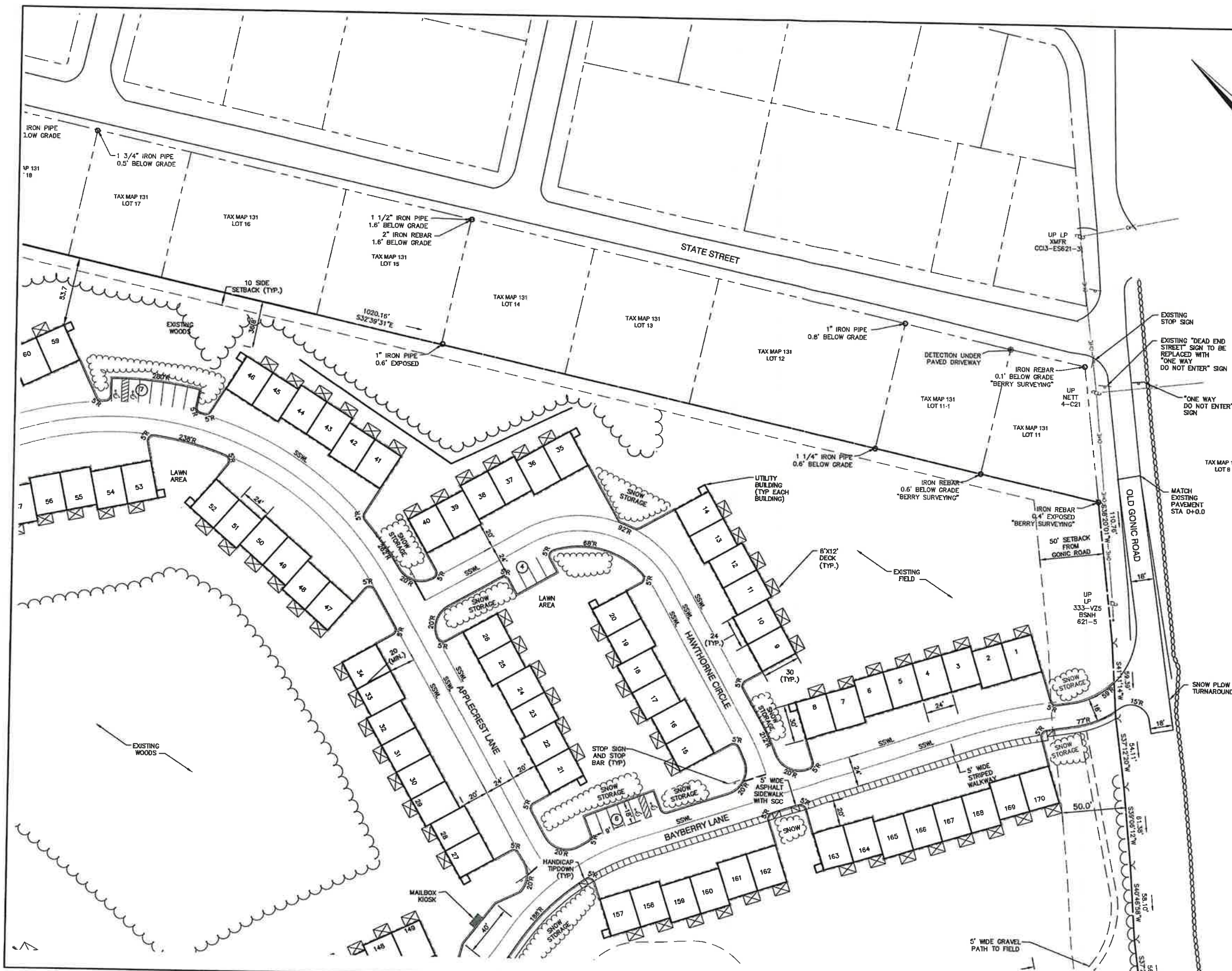


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8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	8/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
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Designed and Produced in NH
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E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **OVERVIEW SITE PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
OVRS
SHEET 8 OF 46
JBE PROJECT NO. 21090



APPROVED - ROCHESTER, NH PLANNING BOARD		PROJECT PARCEL CITY OF ROCHESTER TAX MAP 131, LOT 10
DATE: _____		APPLICANT GREEN AND COMPANY 11 LAFAYETTE ROAD NORTH HAMPTON, NH 03862
		TOTAL LOT AREA 1,309,695 SQ. FT. ± 30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
 Checked: JAC Scale: AS NOTED Project No.: 21090
 Drawing Name: 21090-PLAN.dwg
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Plan Name:	SITE PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

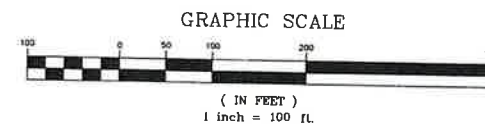
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SHEET 9 OF 46	JBE PROJECT NO. 21090

GRADING AND DRAINAGE NOTES:

- THIS SITE WILL REQUIRE A USEPA NPDES PERMIT FOR STORMWATER DISCHARGE FOR THE CONSTRUCTION SITE. THE CONSTRUCTION SITE OPERATOR SHALL DEVELOP AND IMPLEMENT A CONSTRUCTION STORM WATER POLLUTION PREVENTION PLAN (SWPPP), WHICH SHALL REMAIN ON SITE AND BE MADE ACCESSIBLE TO THE PUBLIC. THE CONSTRUCTION SITE OPERATOR SHALL SUBMIT A NOTICE OF INTENT (NOI) TO THE EPA REGIONAL OFFICE SEVEN DAYS PRIOR TO COMMENCEMENT OF ANY WORK ON SITE. EPA WILL POST THE NOI AT [HTTP://CFPUB1.EPA.GOV/NPEES/STORMWATER/NOI/NOISEARCH.CFM](http://cfpub1.epa.gov/nepes/stormwater/noi/noisearch.cfm). AUTHORIZATION IS GRANTED UNDER THE PERMIT ONCE THE NOI IS SHOWN IN "ACTIVE" STATUS ON THIS WEBSITE. A COMPLETED NOTICE OF TERMINATION SHALL BE SUBMITTED TO THE NPDES PERMITTING AUTHORITY WITHIN 30 DAYS AFTER EITHER OF THE FOLLOWING CONDITIONS HAVE BEEN MET:
 - FINAL STABILIZATION HAS BEEN ACHIEVED ON ALL PORTIONS OF THE SITE FOR WHICH THE PERMITTEE IS RESPONSIBLE; OR
 - ANOTHER OPERATOR/PERMITTEE HAS ASSUMED CONTROL OVER ALL AREAS OF THE SITE THAT HAVE NOT BEEN FINALLY STABILIZED. PROVIDE DPW WITH A COPY OF THE NOTICE OF TERMINATION (NOT).
- IF THIS CONSTRUCTION SITE IS NOT STABILIZED WITH PAVEMENT, A ROAD GRAVEL BASE, 85% MATURE VEGETATION COVER, OR RIPRAP BY OCTOBER 15, THEN THE SITE MUST BE PROTECTED WITH OVER-WINTER STABILIZATION. THE WINTER CONSTRUCTION PERIOD IS FROM OCTOBER 15 THROUGH MAY 15. WINTER EXCAVATION AND EARTHWORK ACTIVITIES SHALL BE LIMITED IN EXTENT AND DURATION, TO MINIMIZE POTENTIAL EROSION AND SEDIMENTATION IMPACTS.
- UNDERGROUND FACILITIES, UTILITIES AND STRUCTURES HAVE BEEN PLOTTED FROM FIELD OBSERVATION AND THEIR LOCATION MUST BE CONSIDERED APPROXIMATE ONLY. NEITHER JONES & BEACH ENGINEERS, INC., NOR ANY OF THEIR EMPLOYEES TAKE RESPONSIBILITY FOR THE LOCATION OF ANY UNDERGROUND STRUCTURES AND/OR UTILITIES NOT SHOWN THAT MAY EXIST. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE ALL UNDERGROUND STRUCTURES AND/OR UTILITIES LOCATED PRIOR TO EXCAVATION WORK BY CALLING 888-DIG-SAFE (888-344-7233).
- ALL BENCHMARKS AND TOPOGRAPHY SHOULD BE FIELD VERIFIED BY THE CONTRACTOR.
- SITE GRADING SHALL NOT PROCEED UNTIL EROSION CONTROL MEASURES HAVE BEEN INSTALLED. SEE CONSTRUCTION SEQUENCE ON SHEET E1.
- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR IS REQUIRED TO HAVE THE PROJECT'S LAND SURVEYOR STAKE OR FLAG CLEARING LIMITS. A MINIMUM OF 48 HOURS NOTICE IS REQUIRED.
- ALL SWALES AND DETENTION PONDS ARE TO BE STABILIZED PRIOR TO DIRECTING RUNOFF TO THEM.
- PROPOSED RIM ELEVATIONS OF DRAINAGE STRUCTURES ARE APPROXIMATE. FINAL ELEVATIONS ARE TO BE SET FLUSH WITH FINISH GRADES.
- ALL SWALES AND ANY SLOPES GREATER THAN 3:1 SHALL BE STABILIZED WITH NORTH AMERICAN GREEN S75 EROSION CONTROL BLANKETS (OR AN EQUIVALENT APPROVED IN WRITING BY THE ENGINEER), UNLESS OTHERWISE SPECIFIED.
- ALL DRAINAGE AND SANITARY STRUCTURE INTERIOR DIAMETERS (4" MIN) SHALL BE DETERMINED BY THE MANUFACTURER BASED ON THE PIPE CONFIGURATIONS SHOWN ON THESE PLANS. CATCH BASINS SHALL HAVE 3" DEEP SUMPS WITH GREASE HOODS, UNLESS OTHERWISE NOTED.
- ALL DRAINAGE STRUCTURES SHALL BE PRECAST, UNLESS OTHERWISE SPECIFIED.
- ALL DRAINAGE STRUCTURES AND STORM SEWER PIPES SHALL MEET HEAVY DUTY TRAFFIC H20 LOADING AND SHALL BE INSTALLED ACCORDINGLY.
- THE CONTRACTOR SHALL INSTALL ORANGE CONSTRUCTION FENCING ALONG PROPERTY LINES AND ALONG WETLAND BUFFERS.
- ALL DRAINAGE PIPE SHALL BE NON-PERFORATED ADS N-12 OR APPROVED EQUAL.
- STONE INLET PROTECTION SHALL BE PLACED AT ALL CATCH BASINS. SEE DETAIL WITHIN THE DETAIL SHEETS.
- LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROVAL TO DO SO HAS BEEN RECEIVED BY ALL GOVERNING AUTHORITIES. THE GENERAL CONTRACTOR SHALL STRICTLY ADHERE TO THE EPA SWPPP DURING CONSTRUCTION OPERATIONS.
- ALL EXPOSED AREAS SHALL BE SEEDED AS SPECIFIED WITHIN 3 DAYS OF FINAL GRADING AND ANYTIME CONSTRUCTION STOPS FOR LONGER THAN 3 DAYS.
- MAINTAIN EROSION CONTROL MEASURES AFTER EACH RAIN EVENT OF 0.5" OR GREATER IN A 24 HOUR PERIOD AND AT LEAST ONCE A WEEK.
- THIS PLAN SHALL NOT BE CONSIDERED ALL INCLUSIVE, AS THE GENERAL CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SEDIMENT FROM LEAVING THE SITE.
- CONSTRUCTION VEHICLES SHALL UTILIZE THE STABILIZED CONSTRUCTION ENTRANCE TO THE EXTENT POSSIBLE THROUGHOUT CONSTRUCTION.
- IF INSTALLATION OF STORM DRAINAGE SYSTEM SHOULD BE INTERRUPTED BY WEATHER OR NIGHTFALL, THE PIPE ENDS SHALL BE COVERED WITH FILTER FABRIC.
- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO TAKE WHATEVER MEANS NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION.
- SEDIMENT SHALL BE REMOVED FROM ALL SEDIMENT BASINS BEFORE THEY ARE 25% FULL.
- ALL WORK SHALL BE DONE IN STRICT ACCORDANCE WITH PROJECT SPECIFICATIONS.
- ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED, IF DEEMED NECESSARY BY ON-SITE INSPECTION BY ENGINEER AND/OR REGULATORY OFFICIALS.
- SEE ALSO EROSION AND SEDIMENT CONTROL SPECIFICATIONS ON SHEET E1.
- PRIOR TO CLEARING OR GRADING DISTURBANCE, THE CONTRACTOR SHALL IDENTIFY ALL AREAS OF TYPE 2 INVASIVE SPECIES AS DEFINED BY NHDOT AND ADHERE TO THE PRACTICES OUTLINED IN BEST MANAGEMENT PRACTICES FOR THE CONTROL OF INVASIVE AND NOXIOUS PLANT SPECIES, NHDOT, 2018. THESE PRACTICES SHALL BE FOLLOWED FOR THE ENTIRE CONSTRUCTION TERM INCLUDING ESTABLISHMENT OF LANDSCAPING. AS THE SITE RE-VEGETATES AFTER CONSTRUCTION, LANDSCAPING CONTRACTOR TO INFORM THE OWNER IF ANY INVASIVE SPECIES START TO GROW. OWNER SHALL CONTACT A QUALIFIED REMOVAL COMPANY AND FOLLOW NHDES BEST MANAGEMENT PRACTICES.

SEE SHEET C9
SEE SHEET C10

SEE SHEET C11
SEE SHEET C10



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,895 SQ. FT. ±
30.07 ACRES ±

DRAWING NO.

OVRG

SHEET 13 OF 46
JBE PROJECT NO. 21090

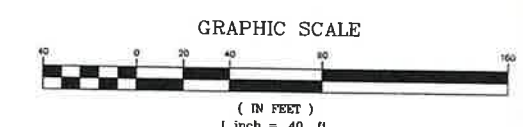
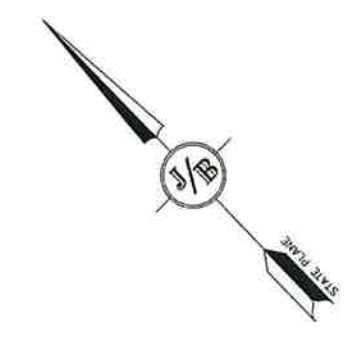
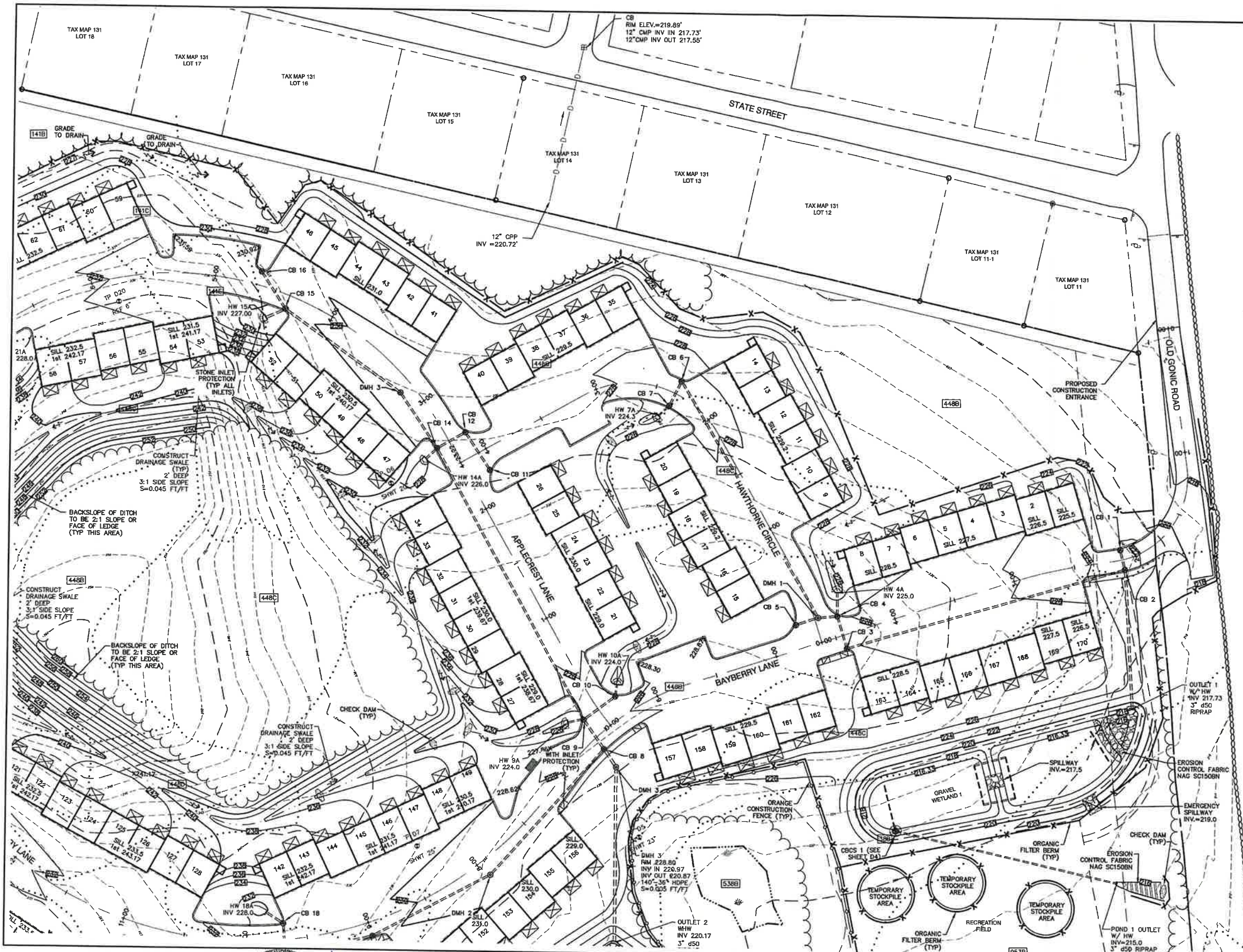
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Plan Name: **OVERVIEW GRADING PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148



PROJECT PARCEL CITY OF ROCHESTER TAX MAP 131, LOT 10
APPLICANT GREEN AND COMPANY 11 LAFAYETTE ROAD NORTH HAMPTON, NH 03862
TOTAL LOT AREA 1,309,695 SQ. FT. ± 30.07 ACRES ±

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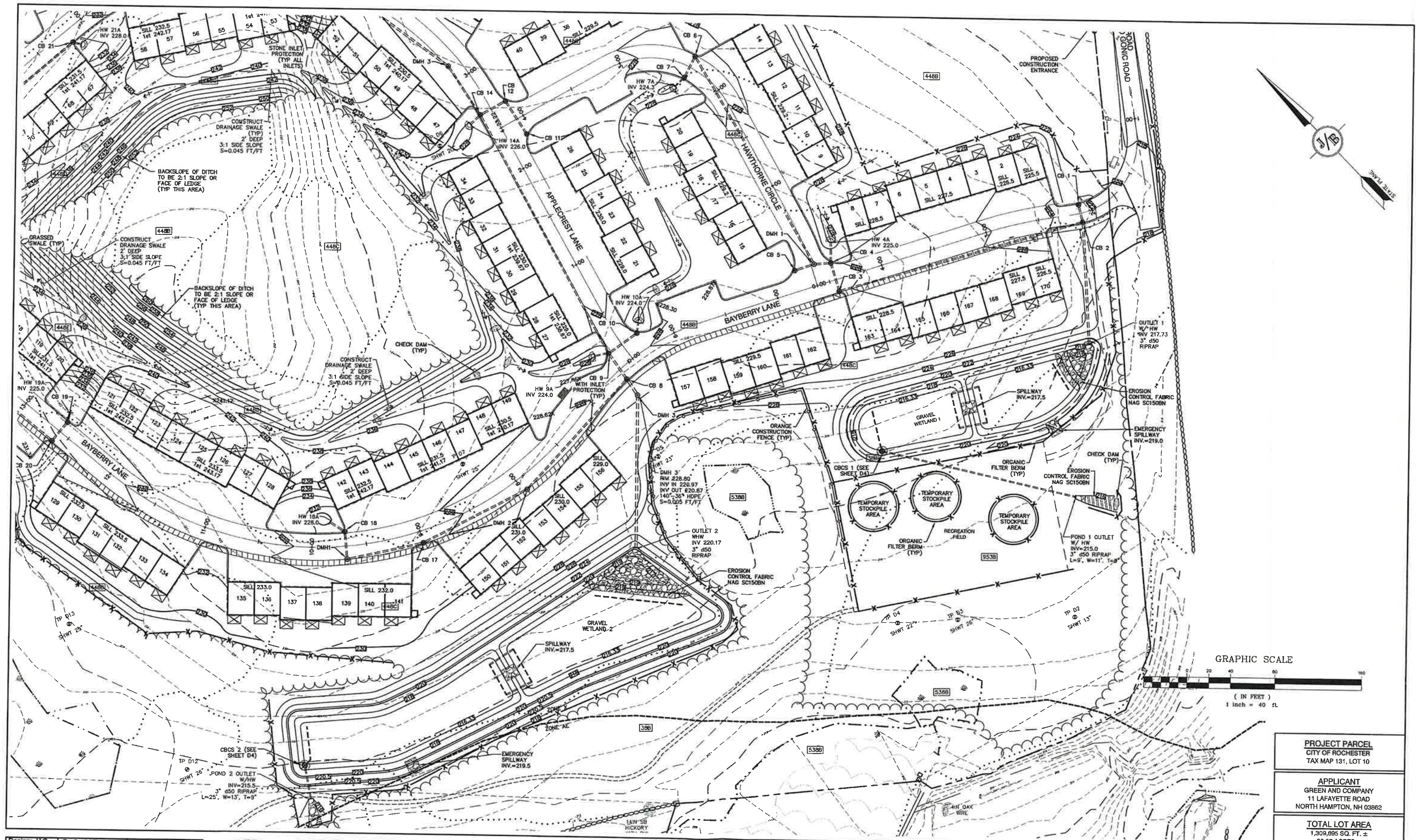


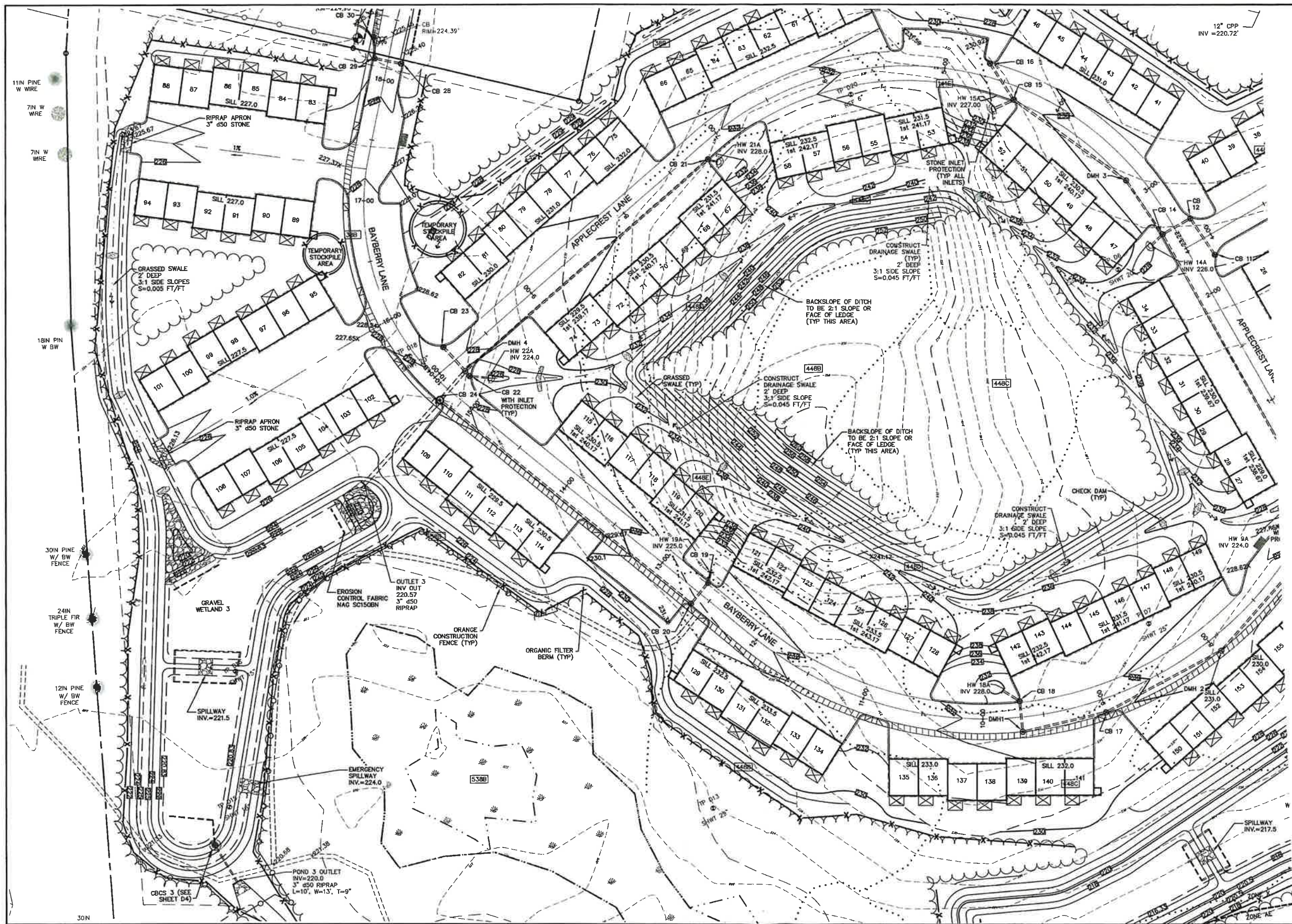
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Plan Name:	GRADING & DRAINAGE PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
C9
 SHEET 14 OF 46
 JBE PROJECT NO. 21090





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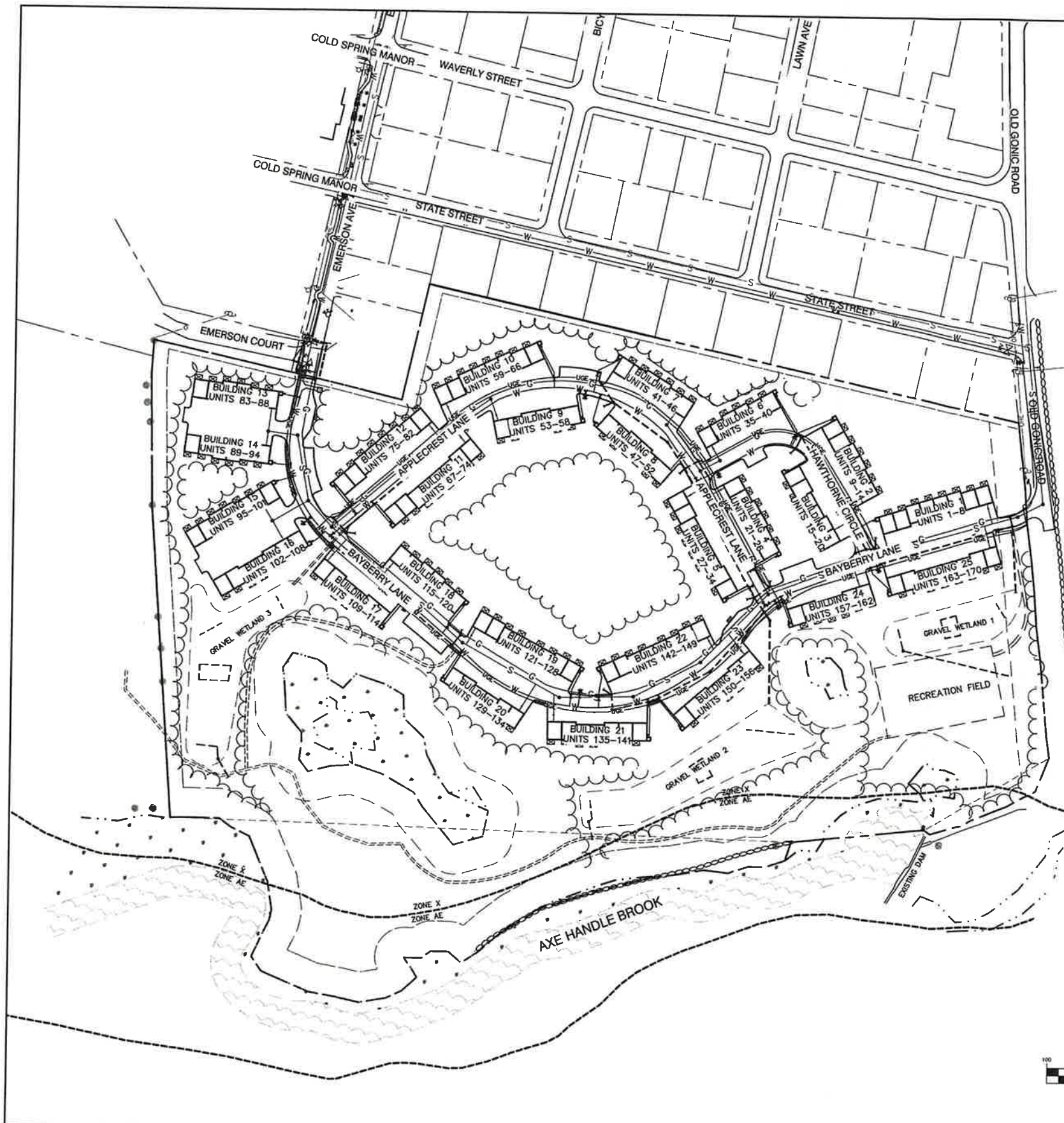


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Plan Name: **GRADING & DRAINAGE PLAN**
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19 OLD GONIC ROAD, ROCHESTER, NH**
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
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DRAWING No.
C11
SHEET 16 OF 46
JBE PROJECT NO. 21090

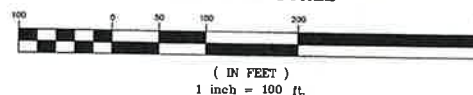


UTILITY NOTES:

- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER, ARCHITECT AND/OR OWNER, IN ORDER TO OBTAIN AND/OR PAY ALL THE NECESSARY LOCAL PERMITS, CONNECTION FEES AND BONDS.
- THE CONTRACTOR SHALL PROVIDE A MINIMUM NOTICE OF FOURTEEN (14) DAYS TO ALL CORPORATIONS, COMPANIES AND/OR LOCAL AUTHORITIES OWNING OR HAVING A JURISDICTION OVER UTILITIES RUNNING TO, THROUGH OR ACROSS PROJECT AREAS PRIOR TO DEMOLITION AND/OR CONSTRUCTION ACTIVITIES.
- THE LOCATION, SIZE, DEPTH AND SPECIFICATIONS FOR CONSTRUCTION OF PROPOSED PRIVATE UTILITY SERVICES SHALL BE TO THE STANDARDS AND REQUIREMENTS OF THE RESPECTIVE UTILITY COMPANY (ELECTRIC, TELEPHONE, CABLE TELEVISION, FIRE ALARM, GAS, WATER, AND SEWER).
- A PRECONSTRUCTION MEETING SHALL BE HELD WITH THE OWNER, ENGINEER, ARCHITECT, CONTRACTOR, LOCAL OFFICIALS, AND ALL PROJECT-RELATED UTILITY COMPANIES (PUBLIC AND PRIVATE) PRIOR TO START OF CONSTRUCTION.
- ALL CONSTRUCTION SHALL CONFORM TO THE CITY STANDARDS AND REGULATIONS, AND NHDES STANDARDS AND SPECIFICATIONS, WHICHEVER ARE MORE STRINGENT, UNLESS OTHERWISE SPECIFIED.
- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO LABOR OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) RULES AND REGULATIONS.
- BUILDING TO BE SERVICED BY UNDERGROUND UTILITIES UNLESS OTHERWISE NOTED.
- THE CONTRACTOR IS TO VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITY STUBS PRIOR TO CONSTRUCTION AND DISCONNECT ALL EXISTING SERVICE CONNECTIONS AT THEIR RESPECTIVE MAINS IN ACCORDANCE WITH THE RESPECTIVE UTILITY COMPANY'S STANDARDS AND SPECIFICATIONS. ENGINEER TO BE NOTIFIED.
- AS-BUILT PLANS SHALL BE SUBMITTED TO DEPARTMENT OF PUBLIC WORKS.
- INVERTS AND SHELVES: MANHOLES SHALL HAVE A BRICK PAVED SHELF AND INVERT, CONSTRUCTED TO CONFORM TO THE SIZE OF PIPE AND FLOW AT CHANGES IN DIRECTION. THE INVERTS SHALL BE LAID OUT IN CURVES OF THE LONGEST RADIUS POSSIBLE TANGENT TO THE CENTER LINE OF THE SEWER PIPES. SHELVES SHALL BE CONSTRUCTED TO THE ELEVATION OF THE THROUGH CHANNEL UNDERLAYMENT OF INVERT, AND SHELF SHALL CONSIST OF BRICK MASONRY.
- FRAMES AND COVERS: MANHOLE FRAMES AND COVERS SHALL BE OF HEAVY DUTY DESIGN AND PROVIDE A 30 INCH DIA. CLEAR OPENING. THE WORD "SEWER" OR "DRAIN" SHALL BE CAST INTO THE CENTER OF THE UPPER FACE OF EACH COVER WITH RAISED, 3" LETTERS.
- SHALLOW MANHOLE: IN LIEU OF A CONE SECTION, WHEN MANHOLE DEPTH IS LESS THAN 6 FEET, AND CAPABLE OF SUPPORTING H2O LOADS.
- CONTRACTOR SHALL PLACE 2" WIDE METAL WIRE IMPREGNATED RED PLASTIC WARNING TAPE OVER ENTIRE LENGTH OF ALL GRAVITY SEWERS, SERVICES, AND FORCE MAINS.
- SANITARY SEWER FLOW CALCULATIONS:
170-THREE BEDROOM UNITS @2000 GPD PER UNIT = 34,000 GPD PER WESTON AND SAMPSON CALCULATIONS.
PEAKING FACTOR = 34,000 GPD X 6 (PEAKING FACTOR) = 204,000 GPD
INFILTRATION (300 GPD PER INCH DIA. X MILE OF PIPE) = 300 X 8 X (2,541 LF/5280) = 1,155 GPD INFILTRATION.
- ALL SANITARY STRUCTURE INTERIOR DIAMETERS (4" MIN) SHALL BE DETERMINED BY THE MANUFACTURER BASED ON THE PIPE CONFIGURATIONS SHOWN ON THESE PLANS.
- PROPOSED RIM ELEVATIONS OF DRAINAGE AND SANITARY MANHOLES ARE APPROXIMATE. FINAL ELEVATIONS ARE TO BE SET FLUSH WITH FINISH GRADES. ADJUST ALL OTHER RIM ELEVATIONS OF MANHOLES, WATER GATES, GAS GATES AND OTHER UTILITIES TO FINISH GRADE AS SHOWN ON THE GRADING AND DRAINAGE PLAN.
- ALL WATER MAINS AND SERVICE PIPES SHALL HAVE A MINIMUM 12" VERTICAL AND 24" HORIZONTAL SEPARATION TO MANHOLES, OR CONTRACTOR SHALL INSTALL BOARD INSULATION FOR FREEZING PROTECTION.
- WATER MAINS SHALL BE HYDROSTATICALLY PRESSURE TESTED FOR LEAKAGE PRIOR TO ACCEPTANCE. WATERMAINS SHALL BE TESTED AT 1.5 TIMES THE WORKING PRESSURE OR 150 PSI, WHICHEVER IS GREATER. TESTING SHALL BE CONDUCTED IN ACCORDANCE WITH SECTION 4 OF AWWA STANDARD C 900. WATERMAINS SHALL BE DISINFECTED AFTER THE ACCEPTANCE OF THE PRESSURE AND LEAKAGE TESTS ACCORDING TO AWWA STANDARD C 651.
- ALL WATER AND SANITARY LEADS TO BUILDING(S) SHALL END 5' OUTSIDE THE BUILDING LIMITS AS SHOWN ON PLANS AND SHALL BE PROVIDED WITH A TEMPORARY PLUG AND WITNESS AT END.
- IF THE BUILDING IS REQUIRED TO HAVE A SPRINKLER SYSTEM, A PRECONSTRUCTION MEETING SHALL BE HELD BETWEEN THE CONTRACTOR, OWNER, ARCHITECT AND THE LOCAL FIRE DEPARTMENT PRIOR TO THE INSTALLATION.
- THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS, TEES, MECHANICAL JOINTS AND FIRE HYDRANTS.
- DIMENSIONS ARE SHOWN TO CENTERLINE OF PIPE OR FITTING.
- REFER TO FIRE PROTECTION SHEETS FOR LOCATION AND DETAIL OF FIRE LINE LEAD IN TO BUILDING. FIRE LINE SHALL BE STUBBED UP 1' ABOVE FINISH FLOOR ELEVATION IN SPRINKLER ROOM. AN APPROVED AUTOMATIC SPRINKLER SYSTEM SHALL BE INSTALLED IN ACCORDANCE WITH 101 LIFE SAFETY CODE/NFPA 1 AND LOCAL REGULATIONS. FIRE DEPARTMENT CONNECTION SHALL BE FIELD VERIFIED BY LOCAL FIRE DEPARTMENT TO ENSURE OPTIMUM PLACEMENT.

- THE CONTRACTOR SHALL HAVE THE APPROVAL OF ALL GOVERNING AGENCIES HAVING JURISDICTION OVER FIRE PROTECTION SYSTEM PRIOR TO INSTALLATION.
- CONTRACTOR TO FURNISH SHOP DRAWINGS FOR UTILITY RELATED ITEMS TO ENSURE CONFORMANCE WITH THE PLANS AND SPECIFICATIONS. SHOP DRAWINGS SHOULD BE SENT IN TRIPPLICATE TO THE DESIGN ENGINEER FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION.
- EXISTING UTILITIES SHALL BE DIGSAFED BEFORE CONSTRUCTION.
- ALL WATER LINES SHOULD HAVE TESTABLE BACKFLOW PREVENTERS AT THE ENTRANCE TO EACH BUILDING.
- ALL GRAVITY SEWER PIPE, MANHOLES, AND FORCE MAINS SHALL BE TESTED ACCORDING TO NHDES STANDARDS OF DESIGN AND CONSTRUCTION FOR SEWAGE AND WASTEWATER TREATMENT FACILITIES, CHAPTER ENV-WQ 700, ADOPTED ON 10-15-14.
- SPECIFICATIONS FOR GRAVITY SEWER PIPE TESTING REQUIREMENTS PER ENV WQ 704.06.
(a) ALL NEW GRAVITY SEWERS SHALL BE TESTED FOR WATER TIGHTNESS BY THE USE OF LOW-PRESSURE AIR TESTS.
(b) LOW-PRESSURE AIR TESTING SHALL BE IN CONFORMANCE WITH THE FOLLOWING TESTING STANDARDS IN EFFECT AT THE TIME THE TEST IS CONDUCTED:
(1) ASTM F1417 "STANDARD TEST METHOD FOR INSTALLATION ACCEPTANCE OF PLASTIC GRAVITY SEWER LINES USING LOW-PRESSURE AIR", AVAILABLE AS NOTED IN APPENDIX D, OR
(2) UN-BELL PVC PIPE ASSOCIATION UN-B-6, "LOW-PRESSURE AIR TESTING OF INSTALLED SEWER PIPE", AVAILABLE AS NOTED IN APPENDIX D.
(c) ALL NEW GRAVITY SEWERS SHALL BE:
(1) CLEANED AND VISUALLY INSPECTED USING A LAMP TEST AND BY INTRODUCING WATER TO DETERMINE THAT THERE IS NO STANDING WATER IN THE SEWER; AND
(2) TRUE TO LINE AND GRADE FOLLOWING INSTALLATION AND PRIOR TO USE.
(d) ALL PLASTIC SEWER PIPE SHALL BE VISUALLY INSPECTED AND DEFLECTION TESTED NOT LESS THAN 30 DAYS NOR MORE THAN 90 DAYS FOLLOWING INSTALLATION.
(e) THE MAXIMUM ALLOWABLE DEFLECTION OF FLEXIBLE SEWER PIPE SHALL BE 5% PERCENT OF AVERAGE INSIDE DIAMETER. A RIGID BALL OR MANDREL WITH A DIAMETER OF AT LEAST 85% OF THE AVERAGE INSIDE PIPE DIAMETER SHALL BE USED FOR TESTING PIPE DEFLECTION. THE DEFLECTION TEST SHALL BE CONDUCTED WITHOUT MECHANICAL PULLING DEVICES.
- ENV-WQ 704.17 SEWER MANHOLE TESTING: SHALL BE TESTED FOR LEAKAGE USING A VACUUM TEST PRIOR TO BACKFILLING AND PLACEMENT OF SHELVES AND INVERTS.
- SANITARY SEWER LINES SHALL BE LOCATED AT LEAST TEN (10) FEET HORIZONTALLY FROM AN EXISTING OR PROPOSED WATER LINE. WHEN A SEWER LINE CROSSES UNDER A WATER LINE, THE SEWER PIPE JOINTS SHALL BE LOCATED AT LEAST 6 FEET HORIZONTALLY FROM THE WATERMAIN. THE SEWER LINE SHALL ALSO MAINTAIN A VERTICAL SEPARATION OF NOT LESS THAN 18 INCHES.
- SEWERS SHALL BE BURIED TO A MINIMUM DEPTH OF 6 FEET BELOW GRADE IN ALL ROADWAY LOCATIONS, AND TO A MINIMUM DEPTH OF 4 FEET BELOW GRADE IN ALL CROSS-COUNTRY LOCATIONS. PROVIDE TWO-INCHES OF R-10 FOAM BOARD INSULATION 2-FOOT WIDE TO BE INSTALLED 6-INCHES OVER SEWER PIPE IN AREAS WHERE DEPTH IS NOT ACHIEVED. A WAIVER FROM THE DEPARTMENT OF ENVIRONMENTAL SERVICES WASTEWATER ENGINEERING BUREAU IS REQUIRED PRIOR TO INSTALLING SEWER AT LESS THAN MINIMUM COVER.
- ALL WATER AND SANITARY LEADS TO BUILDING(S) SHALL END AT RIGHT OF WAY AS SHOWN ON PLANS AND SHALL BE PROVIDED WITH A TEMPORARY PLUG AND WITNESS AT END.
- THE CONTRACTOR SHALL MINIMIZE THE DISRUPTIONS TO THE EXISTING SEWER FLOWS AND THOSE INTERRUPTIONS SHALL BE LIMITED TO FOUR (4) HOURS OR LESS AS DESIGNATED BY THE CITY SEWER DEPARTMENT.
- LIGHTING CONDUIT SHALL BE SCHEDULE 40 PVC, AND SHALL BE INSTALLED IN CONFORMANCE WITH THE NATIONAL ELECTRIC CODE. CONTRACTOR SHALL PROVIDE EXCAVATION AND BACKFILL.
- ALL TRENCHING, PIPE LAYING, AND BACKFILLING SHALL BE IN ACCORDANCE WITH FEDERAL OSHA REGULATIONS.
- DISINFECTION OF WATER MAINS SHALL BE CARRIED OUT IN STRICT ACCORDANCE WITH AWWA STANDARD C651, LATEST EDITION. THE BASIC PROCEDURE TO BE FOLLOWED FOR DISINFECTING WATER MAINS IS AS FOLLOWS:
a. PREVENT CONTAMINATING MATERIALS FROM ENTERING THE WATER MAIN DURING STORAGE, CONSTRUCTION, OR REPAIR.
b. REMOVE, BY FLUSHING OR OTHER MEANS, THOSE MATERIALS THAT MAY HAVE ENTERED THE WATER MAINS.
c. CHLORINATE ANY RESIDUAL CONTAMINATION THAT MAY REMAIN, AND FLUSH THE CHLORINATED WATER FROM THE MAIN.
d. PROTECT THE EXISTING DISTRIBUTION SYSTEM FROM BACKFLOW DUE TO HYDROSTATIC PRESSURE TEST AND DISINFECTION PROCEDURES.
e. DETERMINE THE BACTERIOLOGICAL QUALITY BY LABORATORY TEST AFTER DISINFECTION.
f. MAKE FINAL CONNECTION OF THE APPROVED NEW WATER MAIN TO THE ACTIVE DISTRIBUTION SYSTEM.

GRAPHIC SCALE



Design: JAC | Draft: LAZ | Date: 04/29/21
Checked: JAC | Scale: AS NOTED | Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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10	9/8/22	REVISED PER CITY & NHDES Aot COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES Aot COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ
REV.	DATE	REVISION	BY

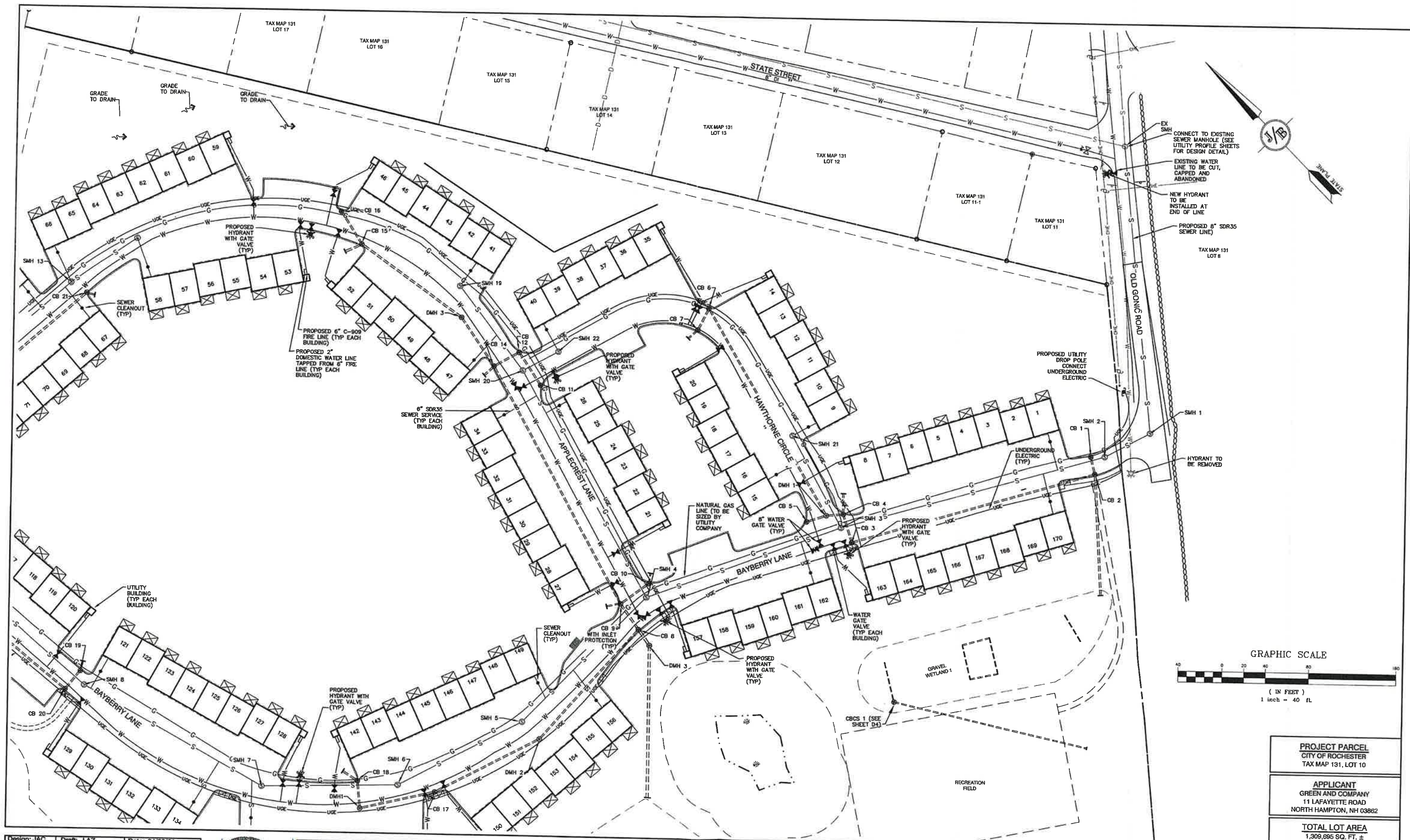
J/B Jones & Beach Engineers, Inc.
Designed and Produced in NH
85 Portsmouth Ave. | Civil Engineering Services | 603-772-4748
PO Box 219 | Stratham, NH 03885 | FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **OVERVIEW UTILITY PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03887 BK 4093 PG 148

DRAWING No.

OVRU

SHEET 17 OF 48
JBE PROJECT NO. 21090



Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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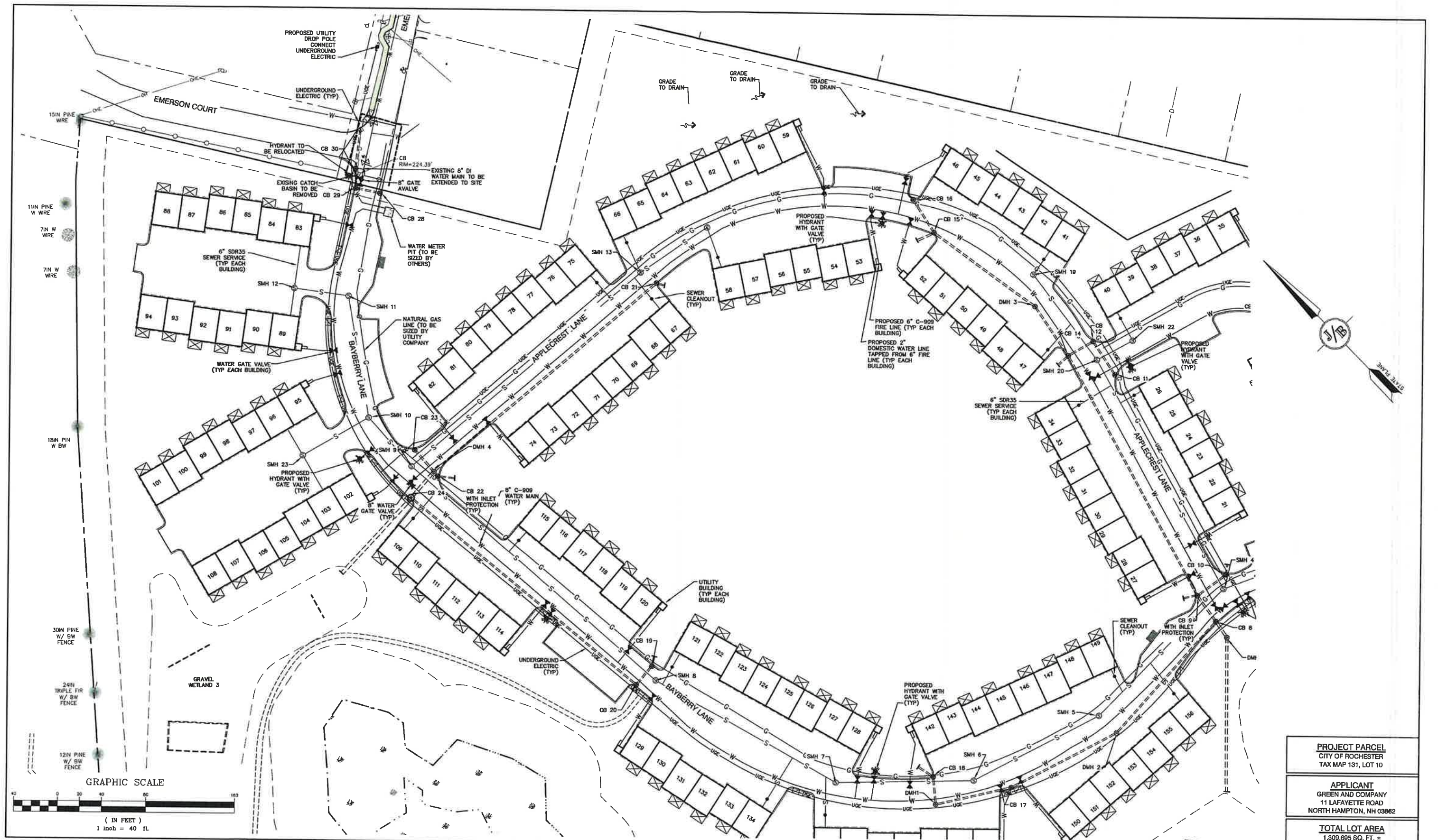


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9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

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Civil Engineering Services
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PO Box 219
Stratham, NH 03885
603-772-4746
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **UTILITY PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONICK ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONICK RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
U1
SHEET 18 OF 46
JBE PROJECT NO. 21090



Design: JAC Draft: LAZ Date: 04/29/21
 Checked: JAC Scale: AS NOTED Project No.: 21090
 Drawing Name: 21090-PLAN.dwg
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9	6/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	6/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ
REV.	DATE	REVISION	BY

J/B Jones & Beach Engineers, Inc.
 Civil Engineering Services
 85 Portsmouth Ave. PO Box 219 Stratham, NH 03885
 603-772-4746 FAX: 603-772-0227 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	UTILITY PLAN
Project:	BAYBERRY COMMONS
Owner of Record:	19 OLD GONIC ROAD, ROCHESTER, NH
	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
	19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10
APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862
TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±
DRAWING No.
U2
SHEET 19 OF 46
JBE PROJECT NO. 21090

LANDSCAPE NOTES:

1. THE CONTRACTOR SHALL LOCATE AND VERIFY THE EXISTENCE OF ALL UTILITIES PRIOR TO STARTING WORK.
2. THE CONTRACTOR SHALL SUPPLY ALL PLANT MATERIALS IN QUANTITIES SUFFICIENT TO COMPLETE THE PLANTINGS SHOWN ON THE DRAWINGS.
3. ALL MATERIAL SHALL CONFORM TO THE GUIDELINES ESTABLISHED BY THE CURRENT AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERMEN.
4. PLANTS FURNISHED IN CONTAINERS SHALL HAVE THE ROOTS WELL ESTABLISHED IN THE SOIL MASS AND SHALL HAVE AT LEAST ONE (1) GROWING SEASON. ROOT-BOUND PLANTS OR INADEQUATELY SIZED CONTAINERS TO SUPPORT THE PLANT MAY BE DEEMED UNACCEPTABLE.
5. NO PLANT SHALL BE PUT IN THE GROUND BEFORE GRADING HAS BEEN FINISHED AND APPROVED BY THE LANDSCAPE ARCHITECT.
6. ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE FIRST 24-HOUR PERIOD AFTER PLANTING. ALL PLANTS SHALL BE WATERED WEEKLY, OR MORE OFTEN IF NECESSARY, DURING THE FIRST GROWING SEASON.
7. ALL PLANTS SHALL BE GUARANTEED BY THE CONTRACTOR FOR NOT LESS THAN ONE FULL YEAR FROM THE TIME OF PROVISIONAL ACCEPTANCE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSPECT THE PLANTS TO ENSURE PROPER CARE.
8. BY THE END OF THE GUARANTEE PERIOD, THE CONTRACTOR SHALL HAVE REPLACED ANY PLANT MATERIAL THAT IS MISSING, NOT TRUE TO SIZE AS SPECIFIED, THAT HAS DIED, LOST NATURAL SHAPE DUE TO DEAD BRANCHES, EXCESSIVE PRUNING OR INADEQUATE OR IMPROPER CARE, OR THAT IS, IN THE OPINION OF THE LANDSCAPE ARCHITECT, IN UNHEALTHY OR UNSIGHTLY CONDITION.
9. ALL LANDSCAPE AREAS TO BE GRASS COMMON TO REGION, EXCEPT FOR INTERIOR LANDSCAPED ISLANDS OR WHERE OTHER PLANT MATERIAL IS SPECIFIED.
10. ALL TREES AND SHRUBS SHALL BE PLANTED IN MULCH BEDS WITH EDGE STRIPS TO SEPARATE TURF GRASS AREAS.
11. THE CONTRACTOR SHALL REMOVE WEEDS, ROCKS, CONSTRUCTION ITEMS, ETC. FROM ANY LANDSCAPE AREA SO DESIGNATED TO REMAIN, WHETHER ON OR OFF-SITE. GRASS SEED OR PINE BARK MULCH SHALL BE APPLIED AS DEPICTED ON PLANS.
12. FINISHED GRADES IN LANDSCAPED ISLANDS SHALL BE INSTALLED SO THAT THEY ARE 1" HIGHER THAN THE TOP OF THE SURROUNDING CURB.
13. ALL LANDSCAPING SHALL MEET THE CITY STANDARDS AND REGULATIONS.
14. EXISTING TREES TO REMAIN SHALL BE CLEARLY MARKED PRIOR TO LAND CLEARING ACTIVITIES AND SHALL BE PROTECTED WITH TEMPORARY SNOW FENCING AT THE DROPLINE OF THE TREE. THE CONTRACTOR SHALL NOT STORE VEHICLES OR MATERIALS WITHIN THE LANDSCAPED AREAS. ANY DAMAGE TO EXISTING TREES, SHRUBS OR LAWN SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
15. ALL MULCH AREAS SHALL RECEIVE A 3" LAYER OF SHREDDED PINE BARK MULCH OVER A 10 MIL WEED MAT EQUAL TO 'WEEDBLOCK' BY EASY GARDENER OR DEWITT WEED BARRIER.
16. ALL LANDSCAPED AREAS SHALL HAVE SELECT MATERIALS REMOVED TO A DEPTH OF AT LEAST 9" BELOW FINISH GRADE. THE RESULTING VOID IS TO BE FILLED WITH A MINIMUM OF 9" HIGH-QUALITY SCREENED LOAM AMENDED WITH 3" OF AGED ORGANIC COMPOST.
17. THIS PLAN IS INTENDED FOR LANDSCAPING PURPOSES ONLY. REFER TO CIVIL/SITE DRAWINGS FOR OTHER SITE CONSTRUCTION INFORMATION.

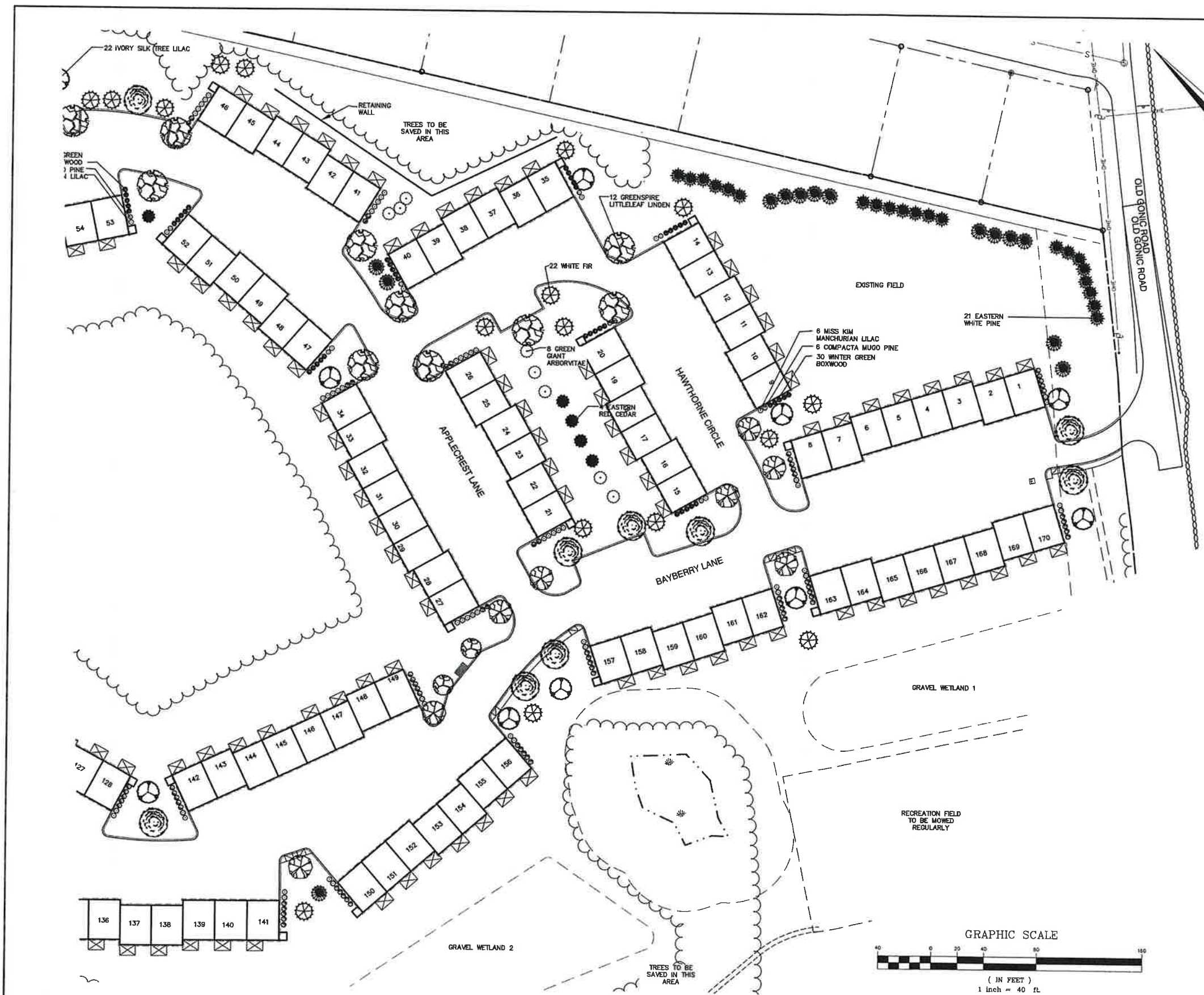
JBE 21090 - OLD GONIC RD TOWNHOUSES

TREES - EVERGREEN & DECIDUOUS

Quantity	Botanical Name	Common Name	Size
22	<i>Abies concolor</i>	WHITE FIR	9-10 FT. HT.
25	<i>Acer saccharum</i> 'Green Mountain'	GREEN MOUNTAIN SUGAR MAPLE	3" Caliper
4	<i>Juniperus virginiana</i>	EASTERN RED CEDAR	7-8 FT. HT.
19	<i>Liquidambar styraciflua</i>	SWEETGUM	3" Caliper
25	<i>Picea abies</i>	NORWAY SPRUCE	9-10 FT. HT.
21	<i>Pinus strobus</i>	EASTERN WHITE PINE	9-10 FT. HT.
4	<i>Prunus serrulata</i> 'Kwanzan'	KWANZAN ORIENTAL CHERRY	2.5" Caliper
22	<i>Syringa reticulata</i> 'Ivory Silk'	IVORY SILK TREE LILAC	2.5" Caliper
8	<i>Thuja plicata</i> 'Green Giant'	GREEN GIANT ARBORVITAE	7-8 FT. HT.
12	<i>Tilia cordata</i> 'Greenspire'	GREENSPIRE LITTLELEAF LINDEN	3" Caliper

SHRUBS - EVERGREEN & DECIDUOUS

88	<i>Buxus microphylla</i> var. <i>koreana</i> 'Winter Green'	WINTER GREEN BOXWOOD	5 Gallon
20	<i>Chamaecyparis platensis</i> 'Aurea'	GOLDEN THREAD CYPRESS	5 Gallon
12	<i>Hydrangea arborescens</i> 'Annabelle'	ANNABELLE HYDRANGEA	5 Gallon
100	<i>Ilex glabra</i> 'Shamrock'	SHAMROCK INKBERY HOLLY	5 Gallon
12	<i>Juniperus chinensis</i> 'Sea Green'	SEA GREEN JUNIPER	5 Gallon
18	<i>Pinus mugo</i> 'Compacta'	COMPACTA MUGO PINE	5 Gallon
80	<i>Rhododendron</i> 'PJM'	PJM RHODODENDRON	5 Gallon
18	<i>Syringa patula</i> 'Miss Kim'	MISS KIM MANCHURIAN LILAC	5 Gallon
20	<i>Weigela florida</i> 'Alexandra'	WINE & ROSES WEIGELA	5 Gallon



Design: JAC Draft: LAZ Date: 04/29/21
 Checked: JAC Scale: AS NOTED Project No.: 21090
 Drawing Name: 21090-PLAN.dwg
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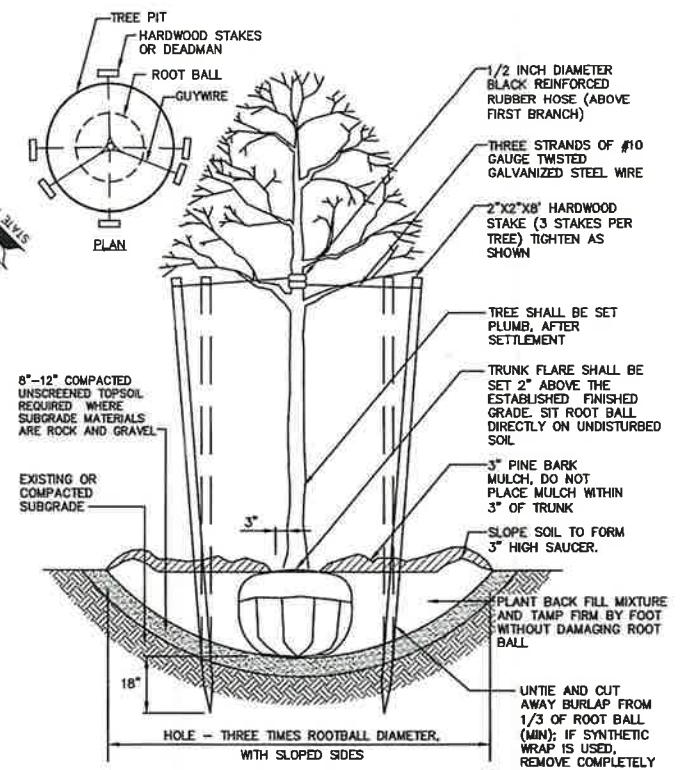
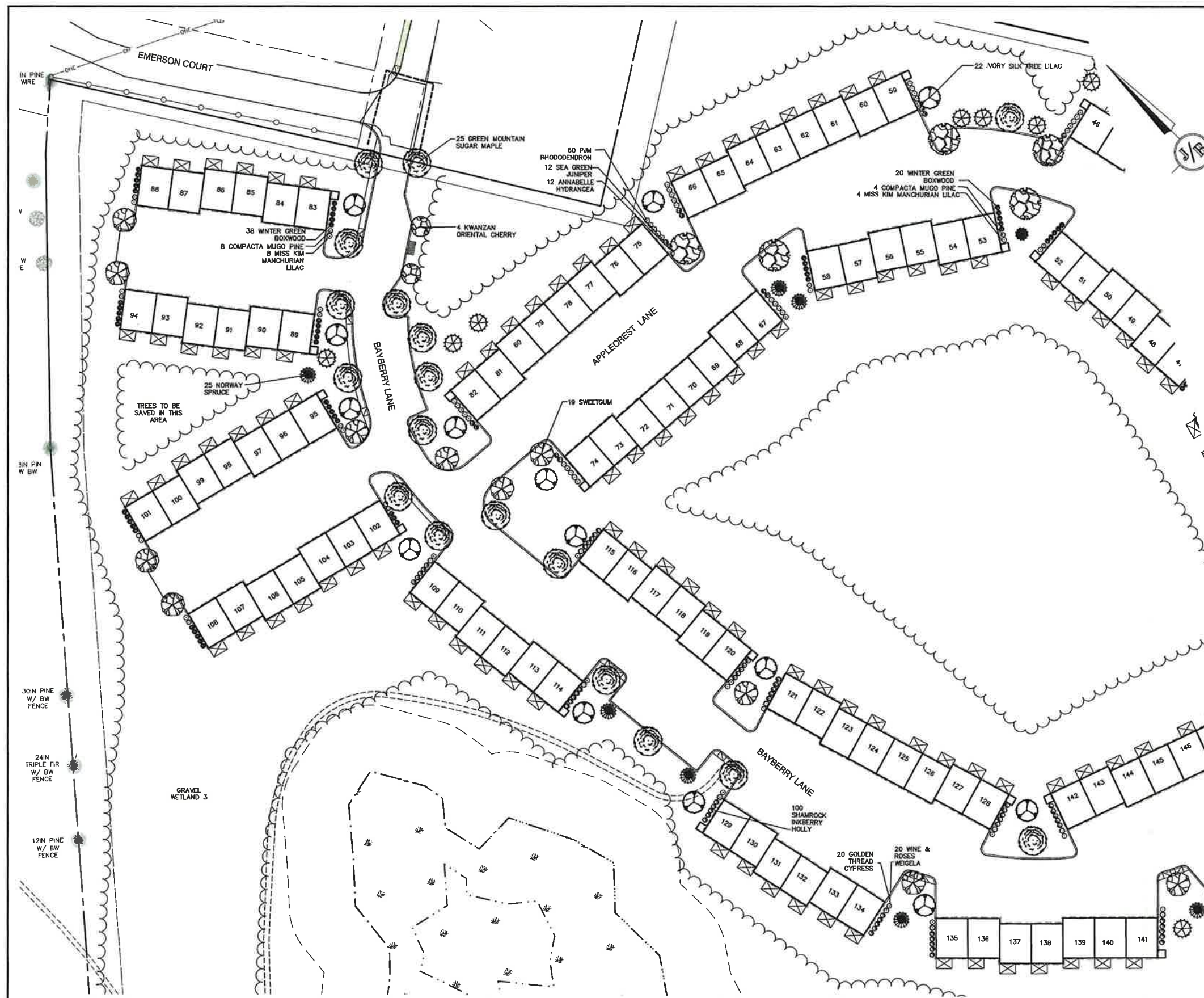


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9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/28/22	REVISED PER TRAFFIC COMMENTS	LAZ
REV.	DATE	REVISION	BY

J/B Jones & Beach Engineers, Inc.
 85 Portsmouth Ave. PO Box 219 Stratham, NH 03885
 Designed and Produced In NH
 Civil Engineering Services
 603-772-4746
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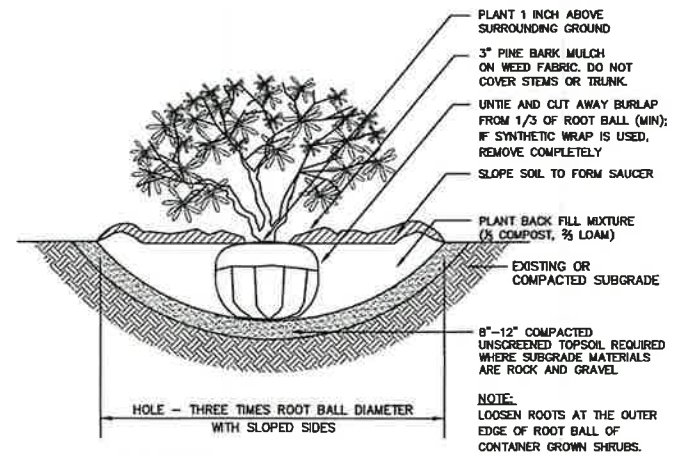
Plan Name: **LANDSCAPE PLAN**
 Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
 Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
L1
 SHEET 20 OF 46
 JBE PROJECT NO. 21090



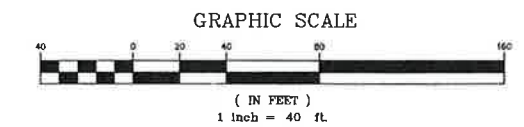
TREE PLANTING (FOR TREES UNDER 4" CALIPER)

NOT TO SCALE



SHRUB PLANTING

NOT TO SCALE



PROJECT PARCEL CITY OF ROCHESTER TAX MAP 131, LOT 10
APPLICANT GREEN AND COMPANY 11 LAFAYETTE ROAD NORTH HAMPTON, NH 03862
TOTAL LOT AREA 1,309,695 SQ. FT. ± 30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
 Checked: JAC Scale: AS NOTED Project No.: 21090
 Drawing Name: 21090-PLAN.dwg
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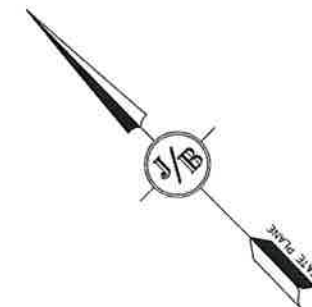
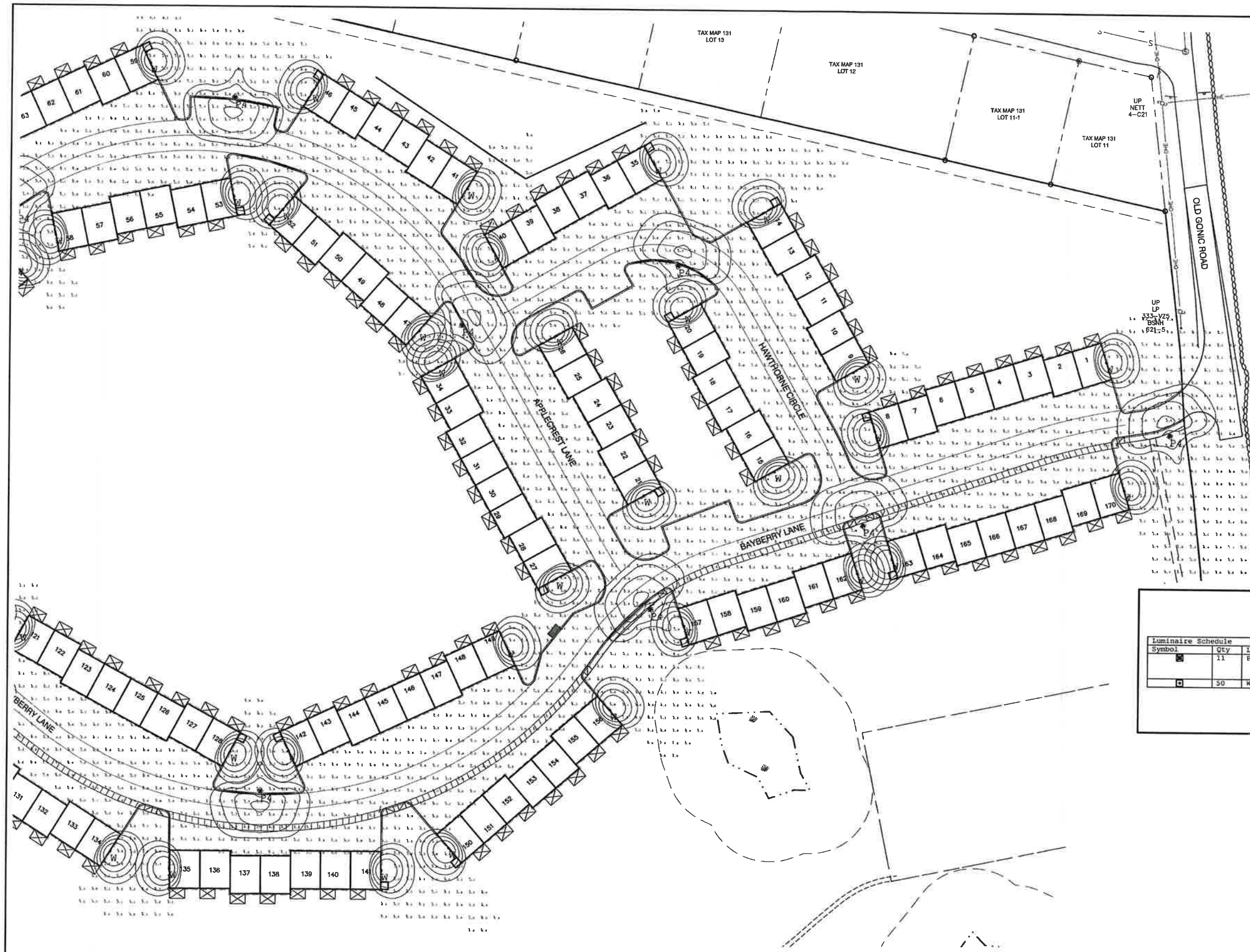


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7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
 85 Portsmouth Ave. PO Box 219 Stratham, NH 03885
 Civil Engineering Services
 603-772-4746 FAX: 603-772-0227 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	LANDSCAPE PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

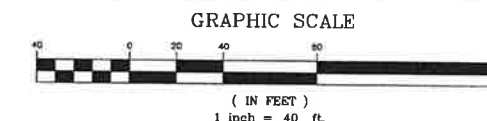
DRAWING No.
L2
 SHEET 21 OF 46
 JBE PROJECT NO. 21090



LIGHTING AND ELECTRICAL NOTES:

1. SITE ELECTRICAL CONTRACTOR SHALL COORDINATE LOCATION OF EASEMENTS, UNDERGROUND UTILITIES AND DRAINAGE BEFORE DRILLING POLE BASES.
2. CONTRACTOR SHALL INSTALL PROPOSED LIGHT POLES ACCORDING TO CITY REGULATIONS.
3. ALL OUTDOOR LIGHTING SYSTEMS SHALL BE EQUIPPED WITH TIMERS TO REDUCE ILLUMINATION LEVELS TO NON-OPERATIONAL VALUES PER CITY REGULATIONS.
4. LIGHTING CONDUIT SHALL BE SCHEDULE 40 PVC, AND SHALL BE INSTALLED IN CONFORMANCE WITH THE NATIONAL ELECTRICAL CODE. CONTRACTOR SHALL PROVIDE EXCAVATION AND BACKFILL.
5. ILLUMINATION READINGS SHOWN ARE BASED ON A TOTAL LLF OF 0.75 AT GRADE. ILLUMINATION READINGS SHOWN ARE IN UNITS OF FOOT-CANDELS.
6. LIGHTING CALCULATIONS SHOWN ARE NOT A SUBSTITUTE FOR INDEPENDENT ENGINEERING ANALYSIS OF LIGHTING SYSTEM AND SAFETY.
7. ALL LIGHTING FIXTURES SHALL BE FULL CUT-OFF DARK-SKY COMPLIANT.
8. THE PROPOSED LIGHTING CALCULATIONS AND DESIGN WAS PERFORMED BY CHARRON, INC., P.O. BOX 4550, MANCHESTER, NH 03108, ATTENTION KEN SWEENEY. ALL LIGHTS SHOULD BE PURCHASED FROM THIS COMPANY, OR AN EQUAL LIGHTING DESIGN SHOULD BE SUBMITTED FOR REVIEW IF EQUAL SUBSTITUTIONS ARE PROPOSED BY THE CONTRACTOR OR OWNER.

Luminaire Schedule				
Symbol	Qty	Label	Arrangement	Description
⬢	11	P4	Single	HER1-FR-40W3K-U-4-N-N-BK / PLB412-4-10-S-125-T300-N-BK (10' POLE)
⬢	50	W	Single	AXCSIAW-PC1 / WALL MTD 12' AFG



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,895 SQ. FT. ±
30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/28/21
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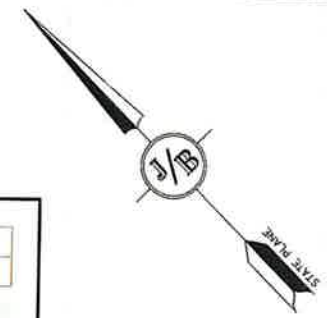
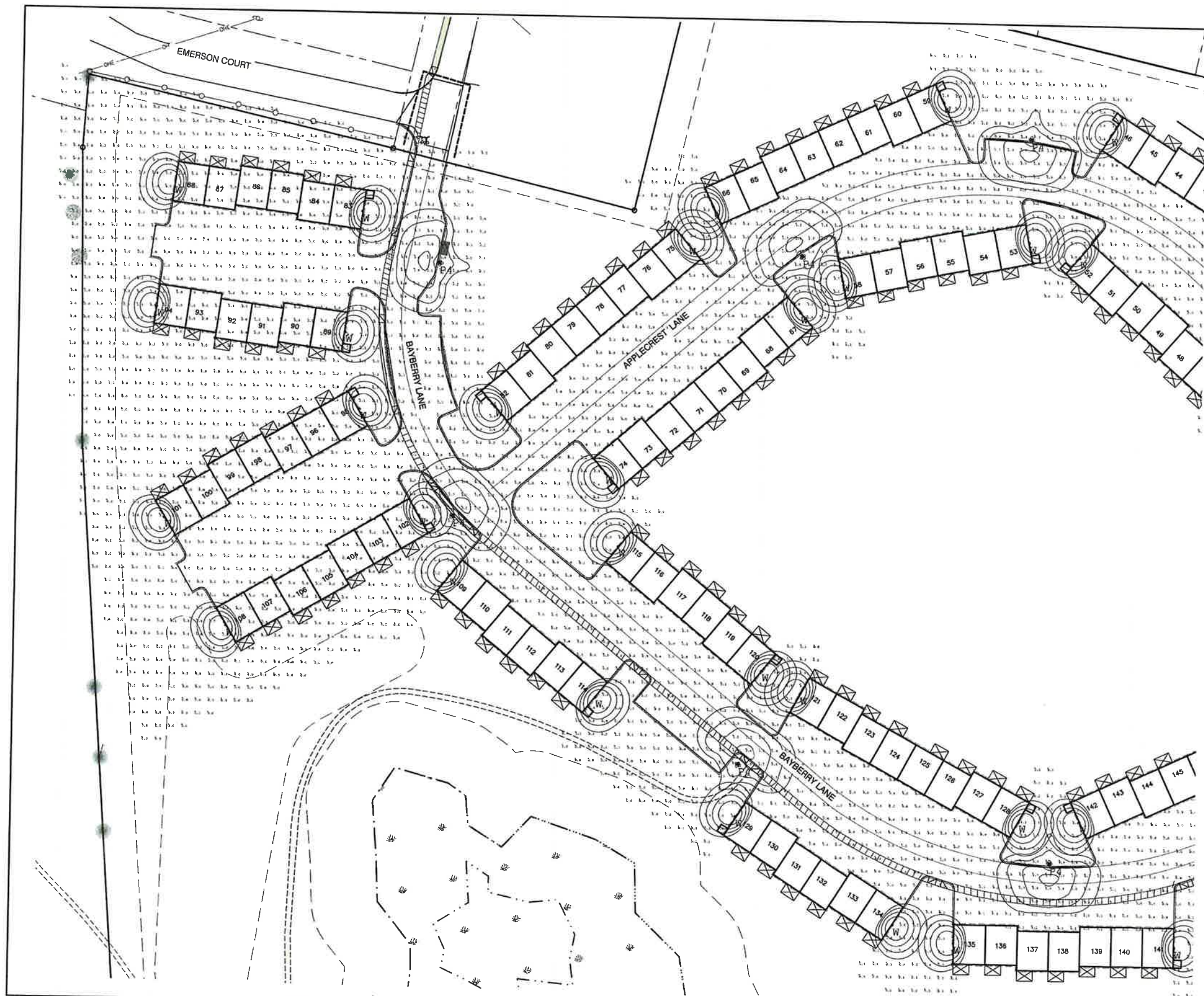


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85 Portsmouth Ave.
PO Box 219
Stratham, NH 03886
603-772-4746
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **LIGHTING PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONICK ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONICK RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
L3
SHEET 22 OF 46
JBE PROJECT NO. 21090



Lumark
Ascent

Well Mount Luminaire

Product Features

- Clearing Information
- Mounting Details
- Product Specifications
- Energy and Performance Data
- Control Options

Product Certifications

- ETL
- UL
- IES
- DLC
- ENEC
- CE
- RoHS
- REACH

Quick Facts

- Available in 14W, 123W (1,200 - 17,000 lumens)
- Probes
- Full control and reflective lens models available
- Energy and maintenance savings up to 90%
- Compliant to IESNA
- Energy efficient illumination results in up to 144 LPW
- Replaces 70W up to 450W HID equivalents

Dimensional Details

COOPER Lighting Systems

PENCO LIGHTING PRODUCTS
A QOSI COMPANY

HERITAGE

RADIANT™ LED POST TOPS

Specifications and Features:

- Radiant™ LED
- Corrosion Resistant LED light engine
- CCT: Warm White (2700K), Cool White (3000K, 3500K, 4000K, 5000K)
- Basic Color: White - Custom Color
- Distribution: • Spot (15°, 30°, 45°, 60°, 75°, 90°) • Flood (15°, 30°, 45°, 60°, 75°, 90°)
- Full Output (No Lens) (15° only)
- Electrical: • Operating Voltage: 120V/277V/347V/480V/500V/600V • 5-10V Dimmable Driver • Standard Surge Protection (included) (10A/15A/20A/30A/40A/50A/60A/70A/80A/90A/100A)
- Controls (Add-on): • Ballast eye photo sensor (PC) • 1pin ballast resistor (PDR) • 1pin ballast resistor (PDR) • 1pin ballast resistor (PDR)
- Housing: • Cast aluminum body, rod and 2" slip fit
- Finish: • Super durable powder coated finish. Custom finish available. Including polished and all RAL colors.
- Listings & Ratings: • ETL listed to UL 1581 approved for wet location • LED is PSE listed
- Warranty: • 5 year limited warranty

GRAPHIC SCALE

1 inch = 40 ft

Design: JAC | Draft: LAZ | Date: 04/29/21
Checked: JAC | Scale: AS NOTED | Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	8/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
Civil Engineering Services

85 Portsmouth Ave.
PO Box 219
Stratham, NH 03885

603-772-4748
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **LIGHTING PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

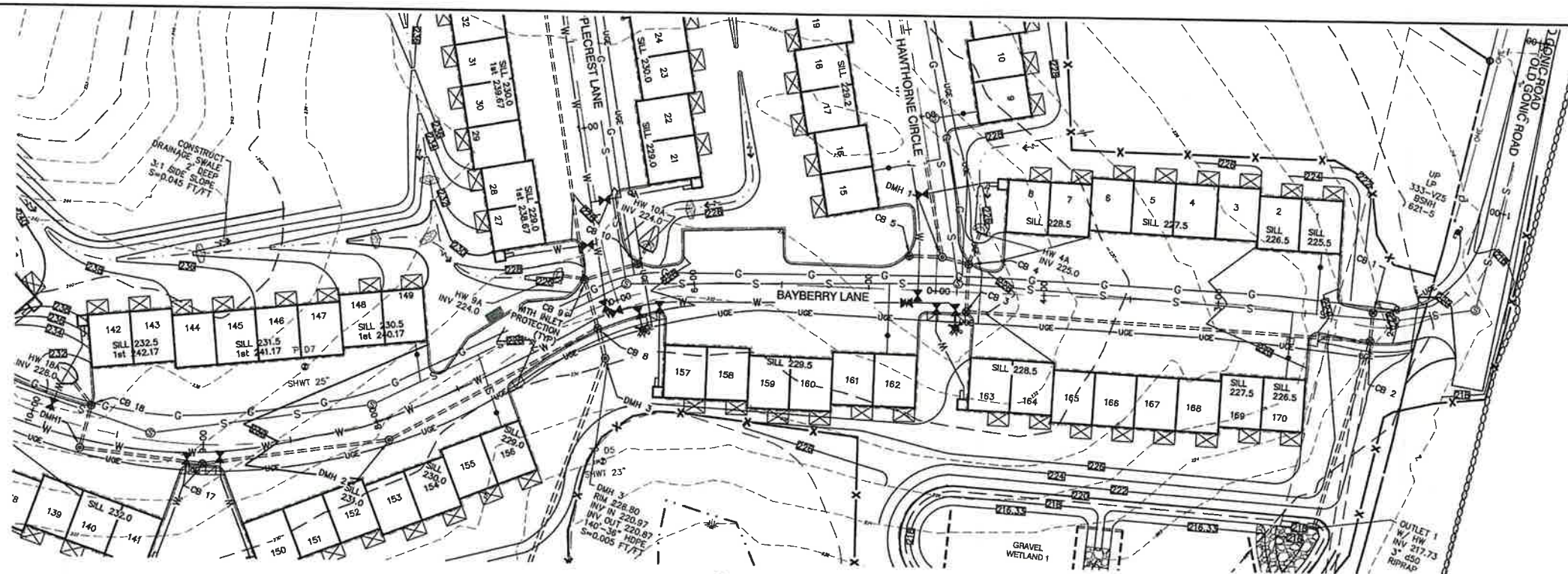
APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

DRAWING No.

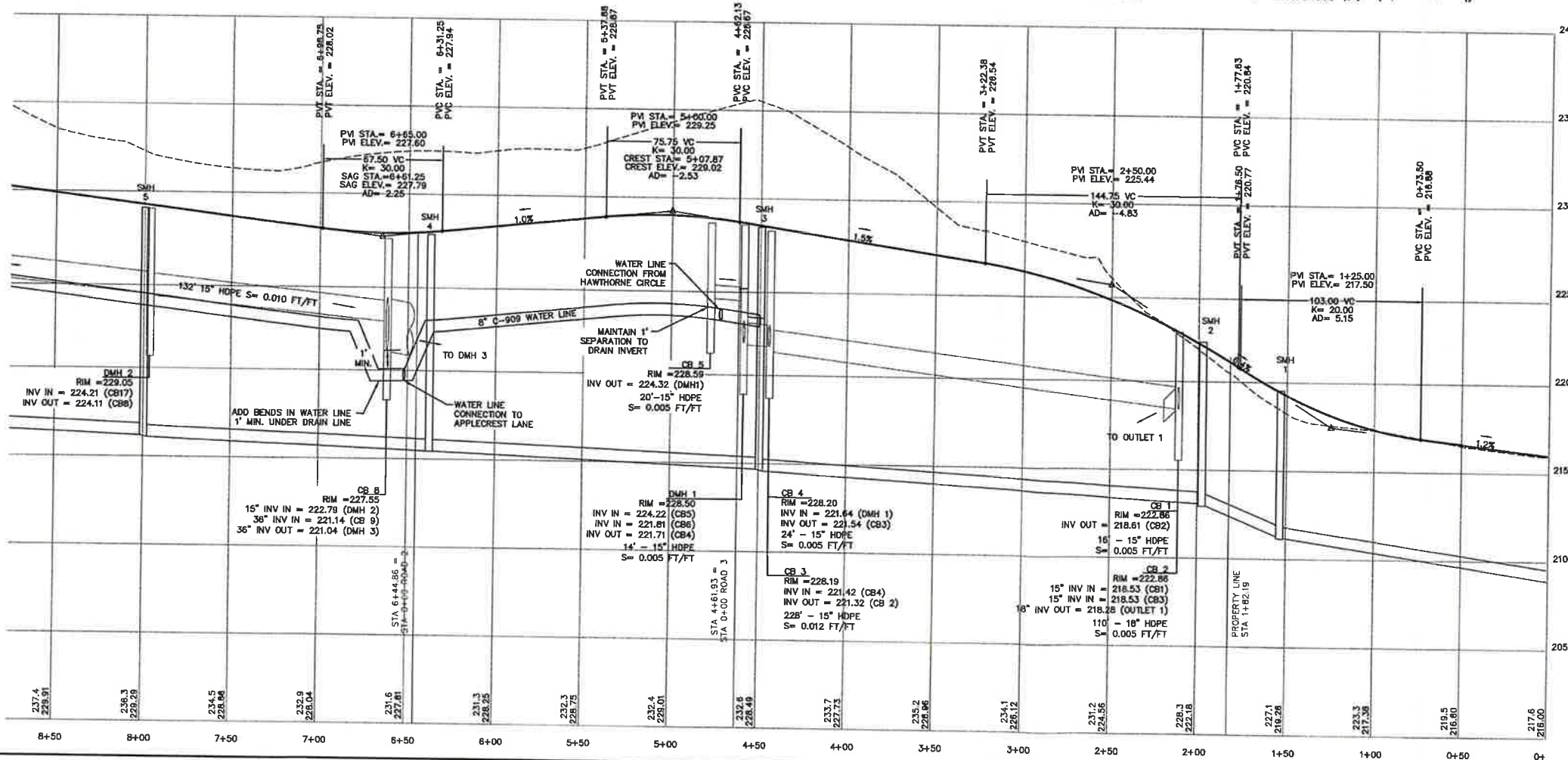
L4

SHEET 23 OF 48
JBE PROJECT NO. 21090



NOTES:

- ALL ROAD AND DRAINAGE WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR THE CITY, AND NHDOT SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, WHICHEVER IS MORE STRINGENT.
- AS-BUILT PLANS TO BE SUBMITTED TO THE CITY PRIOR TO ACCEPTANCE OF THE ROADWAY.
- DEVELOPER IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL WETLAND REGULATIONS, INCLUDING ANY PERMITTING AND SETBACK REQUIREMENTS REQUIRED UNDER THESE REGULATIONS.
- CONTRACTOR TO COORDINATE AND COMPLETE ALL WORK REQUIRED FOR THE RELOCATION AND/OR INSTALLATION OF ELECTRIC, CATV, TELEPHONE, AND FIRE ALARM PER UTILITY DESIGN AND STANDARDS. LOCATIONS SHOWN ARE APPROXIMATE. LOW PROFILE STRUCTURES SHALL BE USED TO THE GREATEST EXTENT POSSIBLE.
- THIS PLAN HAS BEEN PREPARED BY JONES & BEACH ENGINEERS, INC. FOR MUNICIPAL AND STATE APPROVALS AND FOR CONSTRUCTION BASED ON DATA OBTAINED FROM ON-SITE FIELD SURVEY AND EXISTING MUNICIPAL RECORDS. THROUGHOUT THE CONSTRUCTION PROCESS, THE CONTRACTOR SHALL INFORM THE ENGINEER IMMEDIATELY OF ANY FIELD DISCREPANCY FROM DATA SHOWN ON THE DESIGN PLANS. THIS INCLUDES ANY UNFORESEEN CONDITIONS, SUBSURFACE OR OTHERWISE, FOR EVALUATION AND RECOMMENDATIONS. ANY CONTRADICTION BETWEEN ITEMS OF THIS PLAN/PLAN SET, OR BETWEEN THE PLANS AND ON-SITE CONDITIONS MUST BE RESOLVED BEFORE RELATED CONSTRUCTION HAS BEEN INITIATED.
- SILTATION AND EROSION CONTROLS SHALL BE INSTALLED PRIOR TO CONSTRUCTION, SHALL BE MAINTAINED DURING CONSTRUCTION, AND SHALL REMAIN UNTIL SITE HAS BEEN STABILIZED WITH PERMANENT VEGETATION. SEE DETAIL SHEET E1 FOR ADDITIONAL NOTES ON EROSION CONTROL.
- ALL DISTURBED AREAS NOT STABILIZED BY NOVEMBER 1st SHALL BE COVERED WITH AN EROSION CONTROL BLANKET. PRODUCT TO BE SPECIFIED BY THE ENGINEER.
- FINAL DRAINAGE, GRADING AND EROSION PROTECTION MEASURES SHALL CONFORM TO REGULATIONS OF THE PUBLIC WORKS DEPARTMENT.
- CONTRACTOR TO VERIFY EXISTING UTILITIES AND TO NOTIFY ENGINEER OF ANY DISCREPANCY IMMEDIATELY.
- ROADWAY INTERSECTIONS WITH SLOPE GRANITE CURBS SHALL EXTEND AROUND RADIUS WITH 6' STRAIGHT PIECE ALONG TANGENT.
- RETAINING WALLS SHALL BE DESIGNED AND STAMPED BY A LICENSED PROFESSIONAL ENGINEER. CONTRACTOR SHALL COORDINATE WITH MANUFACTURER PRIOR TO INSTALLATION.
- 6" PERFORATED ADS UNDER DRAIN PLACEMENT TO BE DETERMINED BY THE ENGINEER DURING TIME OF SUBGRADE INSPECTION. CONTRACTOR TO ADJUST LOCATION IN THE FIELD ONLY WITH PRIOR APPROVAL OF PROJECT ENGINEER OR PUBLIC WORKS DEPARTMENT.
- ALL DRIVEWAYS TO BE CONSTRUCTED MAXIMUM 10% SLOPE. SEE DETAIL SHEET.
- SIDEWALK TO BE INSTALLED AT TIME OF TOP COURSE PAVING ALONG WITH DRIVEWAY APRONS.
- DRAINAGE INSPECTION AND MAINTENANCE SCHEDULE: SILT FENCING WILL BE INSPECTED DURING AND AFTER STORM EVENTS TO ENSURE THAT THE FENCE STILL HAS INTEGRITY AND IS NOT ALLOWING SEDIMENT TO PASS. SEDIMENT BUILD UP IN SWALES WILL BE REMOVED IF IT IS DEEPER THAN SIX INCHES, AND IS TO BE REMOVED FROM SLOPS BELOW THE INLET OF CULVERTS SEMIANNUALLY, AS WELL AS FROM CATCH BASINS. FOLLOWING MAJOR STORM EVENTS, THE STAGE DISCHARGE OUTLET STRUCTURES ARE TO BE INSPECTED AND ANY DEBRIS REMOVED FROM THE ORIFICE, TRASH TRACK AND EMERGENCY SPILL WAY. INFREQUENTLY, SEDIMENT MAY ALSO HAVE TO BE REMOVED FROM THE SUMP OF THE STRUCTURE.
- ALL DRAINAGE INFRASTRUCTURE SHALL BE INSTALLED AND STABILIZED PRIOR TO DIRECTING ANY RUNOFF TO IT.
- DETENTION PONDS REQUIRE TIMELY MAINTENANCE AND SHOULD BE INSPECTED AFTER EVERY MAJOR STORM EVENT, AS WELL AS FREQUENTLY DURING THE FIRST YEAR OF OPERATION, AND ANNUALLY THEREAFTER. EVERY FIVE YEARS, THE SERVICES OF A PROFESSIONAL ENGINEER SHOULD BE RETAINED TO PERFORM A THOROUGH INSPECTION OF THE DETENTION POND AND ITS INFRASTRUCTURE. ANY DEBRIS AND SEDIMENT ACCUMULATIONS SHOULD BE REMOVED FROM THE OUTLET STRUCTURE(S) AND EMERGENCY SPILLWAY(S) AND DISPOSED OF PROPERLY. DETENTION POND BERMS SHOULD BE MOVED AT LEAST ONCE ANNUALLY SO AS TO PREVENT THE ESTABLISHMENT OF WOODY VEGETATION. TREES SHOULD NEVER BE ALLOWED TO GROW ON A DETENTION POND BERM, AS THEY MAY DESTABILIZE THE STRUCTURE AND INCREASE THE POTENTIAL FOR FAILURE. AREAS SHOWING SIGNS OF EROSION OR THIN OR DYING VEGETATION SHOULD BE REPAIRED IMMEDIATELY BY WHATEVER MEANS NECESSARY, WITH THE EXCEPTION OF FERTILIZER, RODENT BORROWS SHOULD BE REPAIRED IMMEDIATELY AND THE ANIMALS SHOULD BE TRAPPED AND RELOCATED IF THE PROBLEM PERSISTS.
- THE DETENTION PONDS ARE TO BE CONSTRUCTED PRIMARILY THROUGH EXCAVATION, IN THOSE AREAS WHERE THE BERMS MUST BE CONSTRUCTED BY THE PLACEMENT OF FILL, THE ENTIRE EMBANKMENT AREA OF THE DETENTION PONDS SHALL BE EXCAVATED TO PROPOSED GRADE, STRIPPED OF ALL ORGANIC MATERIALS, COMPACTED TO AT LEAST 95% AND SCARIFIED PRIOR TO THE PLACEMENT OF THE EMBANKMENT MATERIAL. IN THE EVENT THE FOUNDATION MATERIAL EXPOSED DOES NOT ALLOW THE SPECIFIED COMPACTION, AN ADDITIONAL ONE FOOT (1') OF EXCAVATION AND THE PLACEMENT OF A ONE FOOT (1') THICK, TWELVE FOOT (12') WIDE PAD OF THE MATERIAL DESCRIBED IN THE NOTE BELOW, COMPACTED TO 95% OF ASTM D-1557 MAY BE NECESSARY. PLACEMENT AND COMPACTION SHOULD OCCUR AT A MOISTURE CONTENT OF OPTIMUM PLUS OR MINUS 3%, AND NO FROZEN OR ORGANIC MATERIAL SHOULD BE PLACED WITHIN FOR ANY REASON.
- EMBANKMENT MATERIAL FOR THE BERMS SHALL BE CLEAN MINERAL SOIL WITH A CLAY COMPONENT FREE OF ROOTS, ORGANIC MATTER, AND OTHER DELETERIOUS SUBSTANCES, AND SHALL CONTAIN NO ROCKS OR LUMPS OVER FOUR INCHES (4") IN DIAMETER. THIS MATERIAL SHOULD BE INSTALLED IN 6" LIFTS AND COMPACTION TO 95% OF ASTM D-1557, AND SHOULD MEET THE FOLLOWING SPECIFICATIONS: 4" PASSING 100%, #4 SIEVE 25-70%, #200 SIEVE 10-25% (N TOTAL SAMPLE).
- EMBANKMENT IS TO HAVE 3:1 SIDE SLOPES (MAX.) AND IS TO BE BROUGHT TO SPECIFIED GRADES PRIOR TO THE ADDITION OF LOAM (4" MINIMUM) SO AS TO ALLOW FOR THE COMPACTION OF THE STRUCTURE OVER TIME WHILE MAINTAINING THE PROPER BERM ELEVATION.
- COMPACTION TESTING SERVICES (I.E. NUCLEAR DENSITY TESTS) ARE TO BE PERFORMED BY AN INDEPENDENT GEOTECHNICAL ENGINEER RETAINED BY THE CONTRACTOR FOR ROADWAY CONSTRUCTION, AND ON THE FOUNDATION OF THE BERM AND ON EVERY LIFT OF NEWLY PLACED MATERIAL.
- SLOPED GRANITE CURBS TO BE TIPPED DOWN AT ALL DRIVEWAY ENTRANCES BY THE CONTRACTOR.



GRAPHIC SCALE
(IN FEET)
1 inch = 40 ft Horiz
1 inch = 4 ft Vert

BAYBERRY LANE

Design: JAC Draft: LAZ Date: 04/29/21
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Drawing Name: 21090-PLAN.dwg
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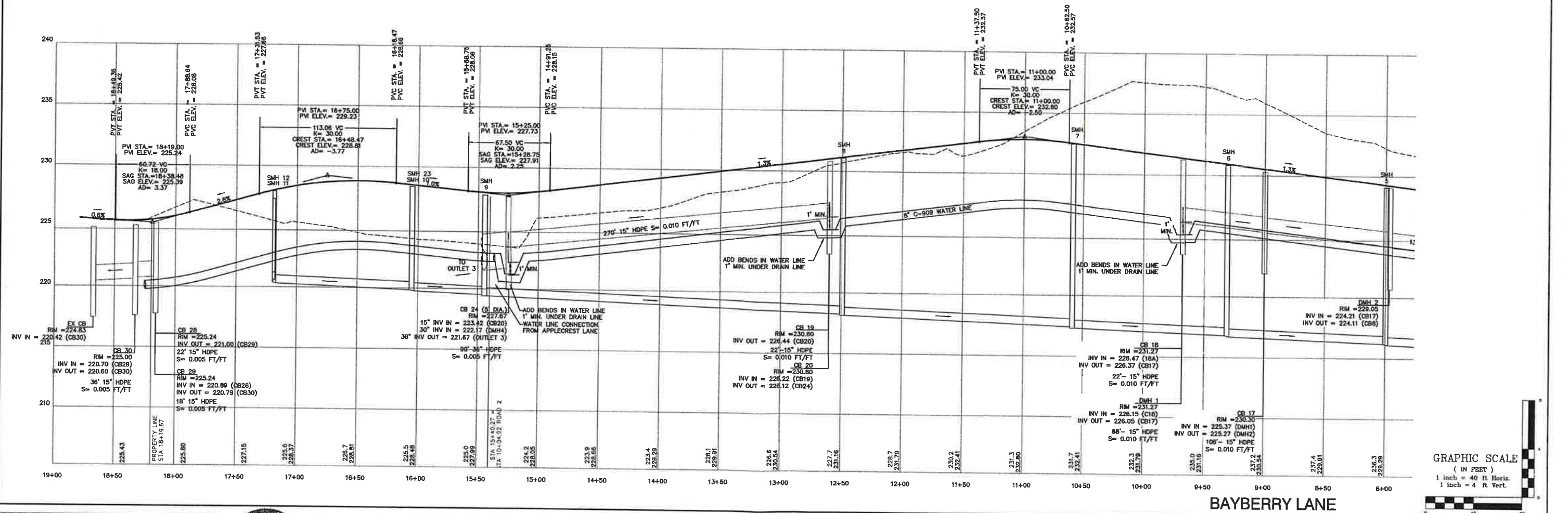
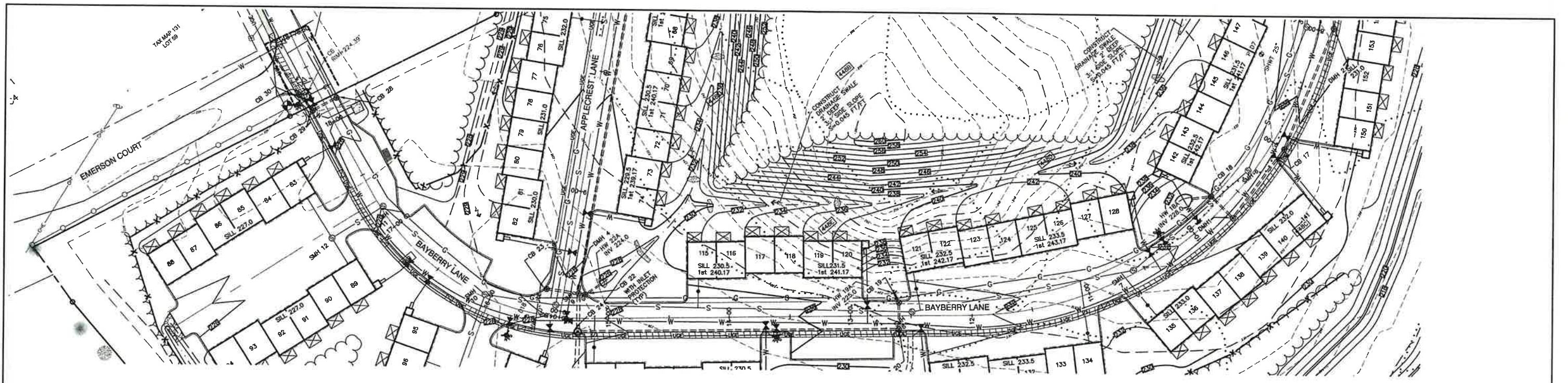


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7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

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E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **PLAN AND PROFILE**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
P1
SHEET 24 OF 46
JBE PROJECT NO. 21090



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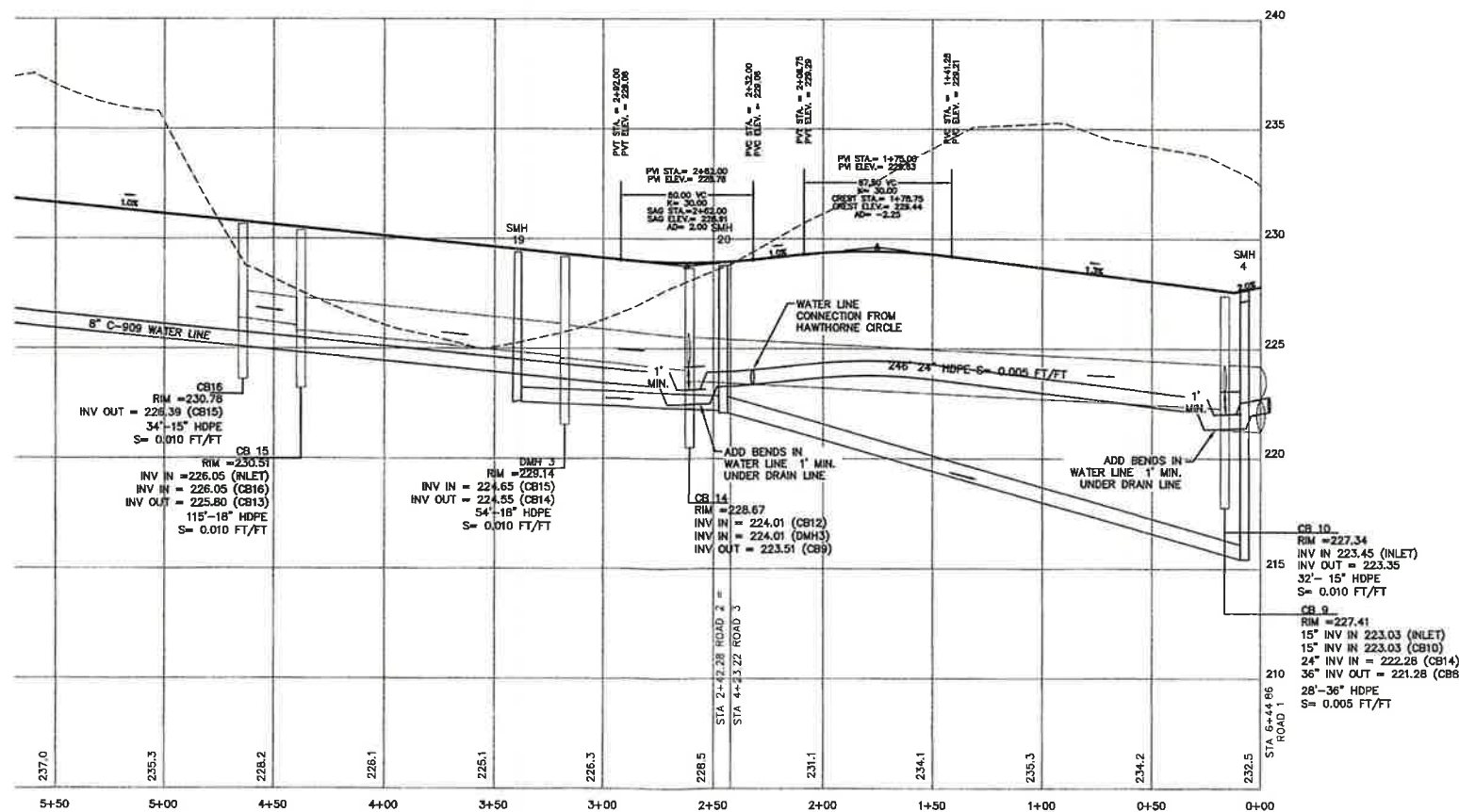
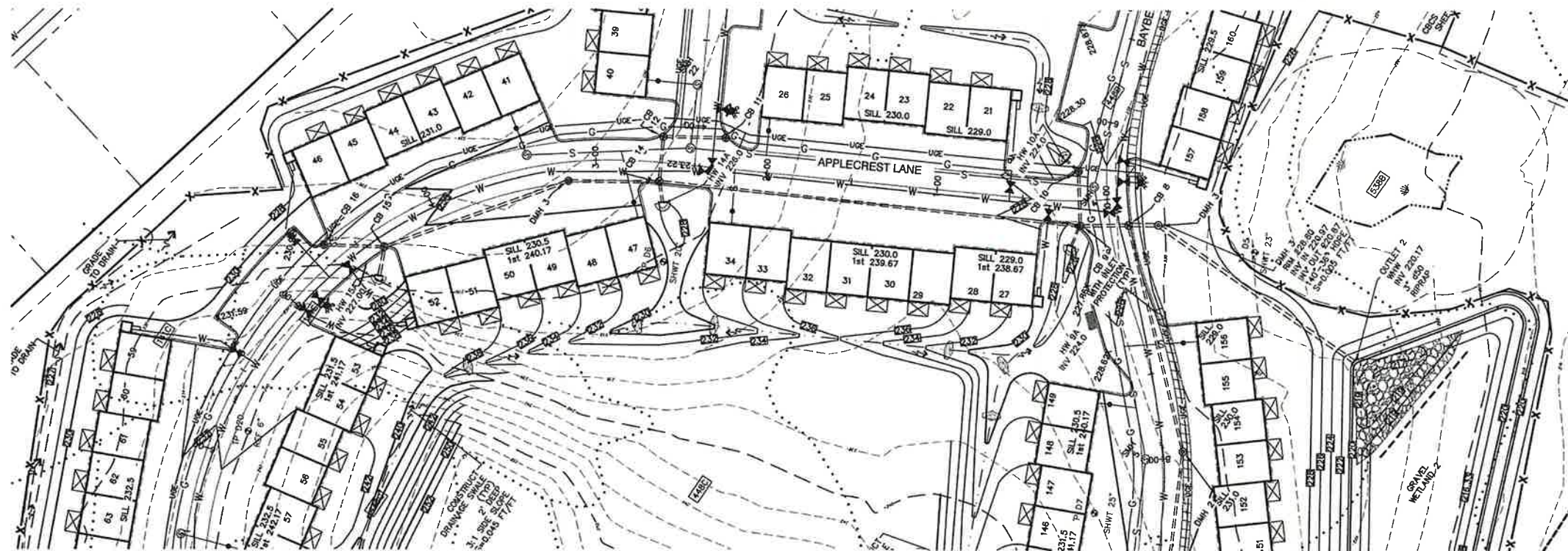


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6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

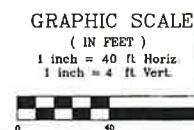
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DRAWING No: **P2**
 SHEET 25 OF 46
 JBE PROJECT NO. 21090



APPLECREST LANE



Design: JAC Draft: LAZ Date: 04/29/21
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 Drawing Name: 21090-PLAN.dwg
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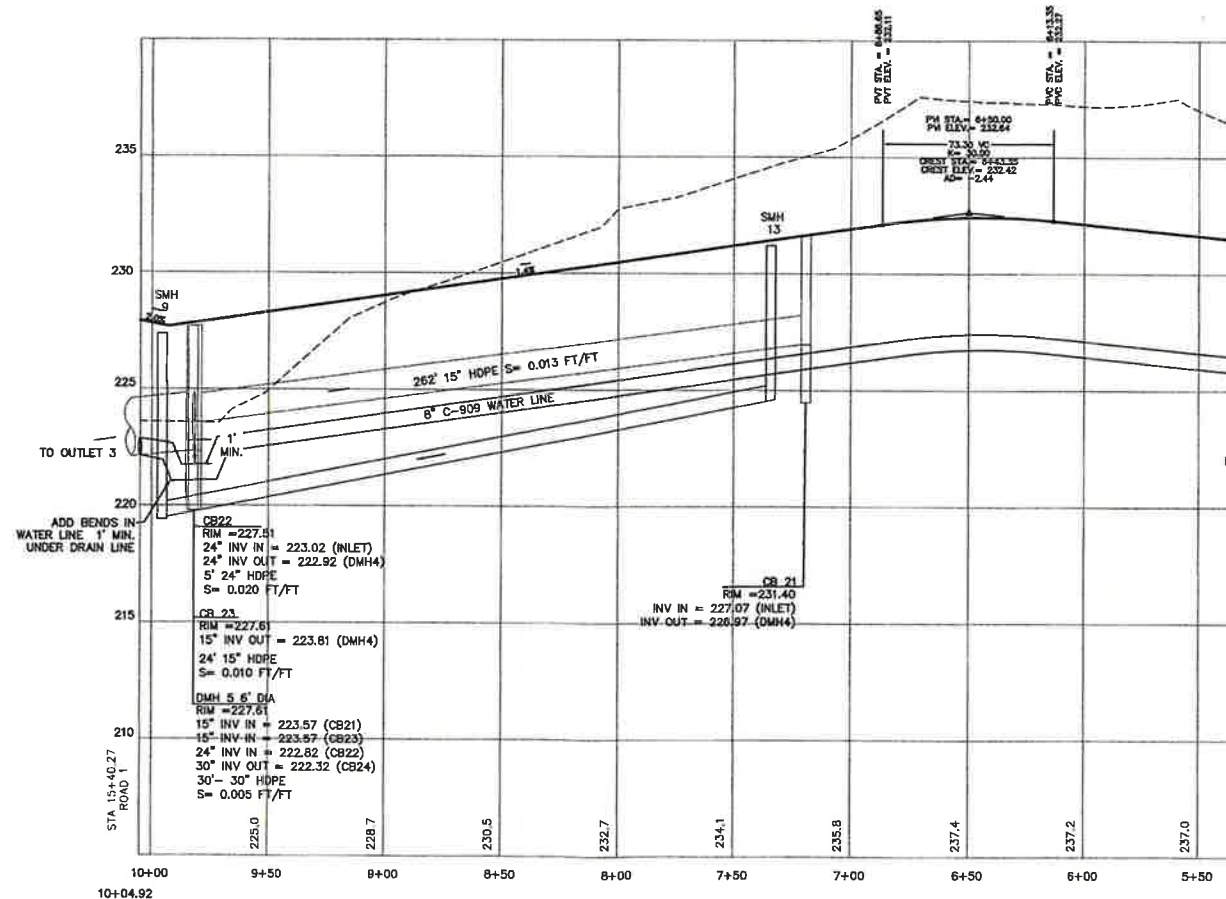
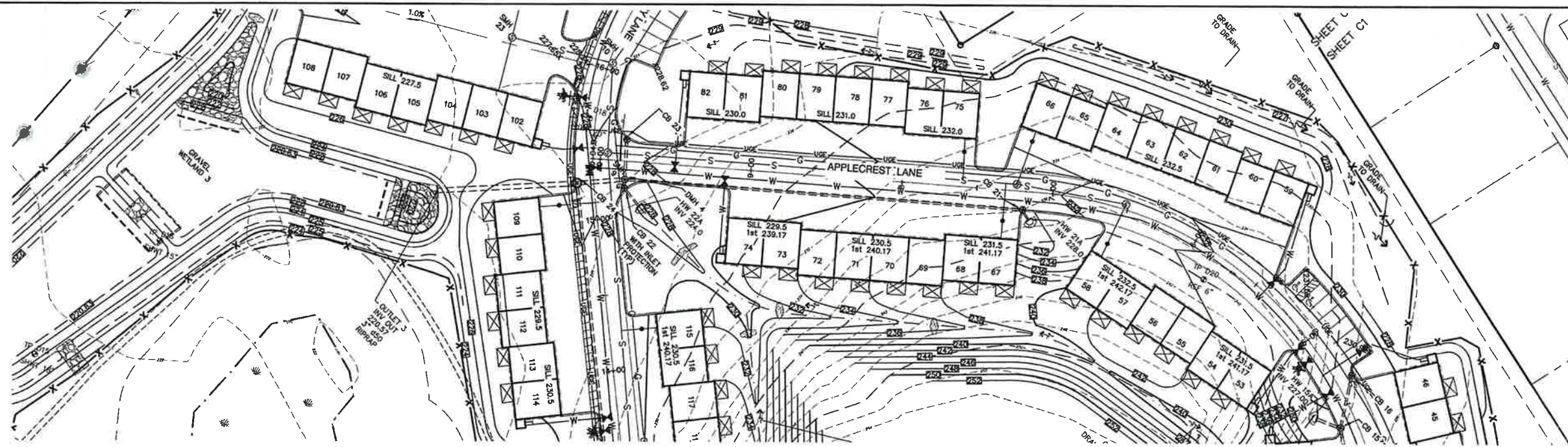


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6	8/29/22	REVISED PER TRAFFIC COMMENTS	LAZ
REV.	DATE	REVISION	BY

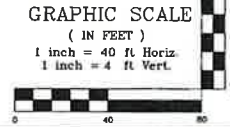
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Plan Name: **PLAN AND PROFILE**
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 Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
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DRAWING No.
P3
 SHEET 26 OF 46
 JBE PROJECT NO. 21090



APPLECREST LANE



Design: JAC Draft: LAZ Date: 04/29/21
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6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

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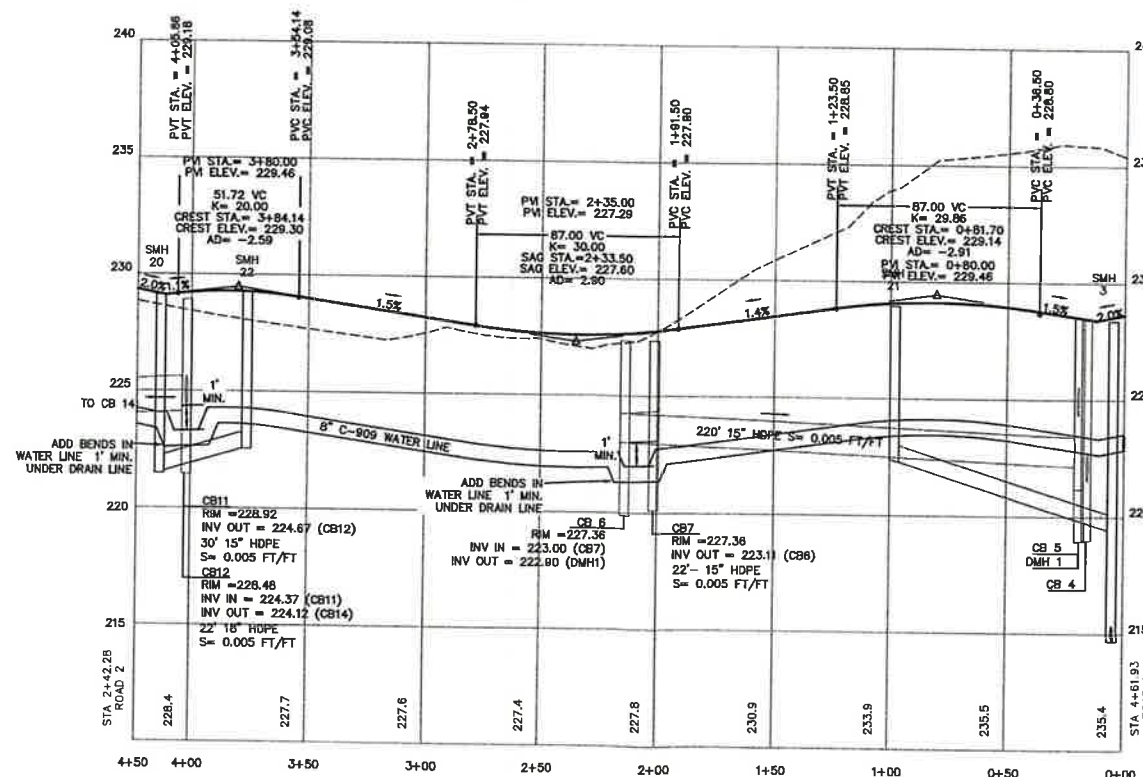
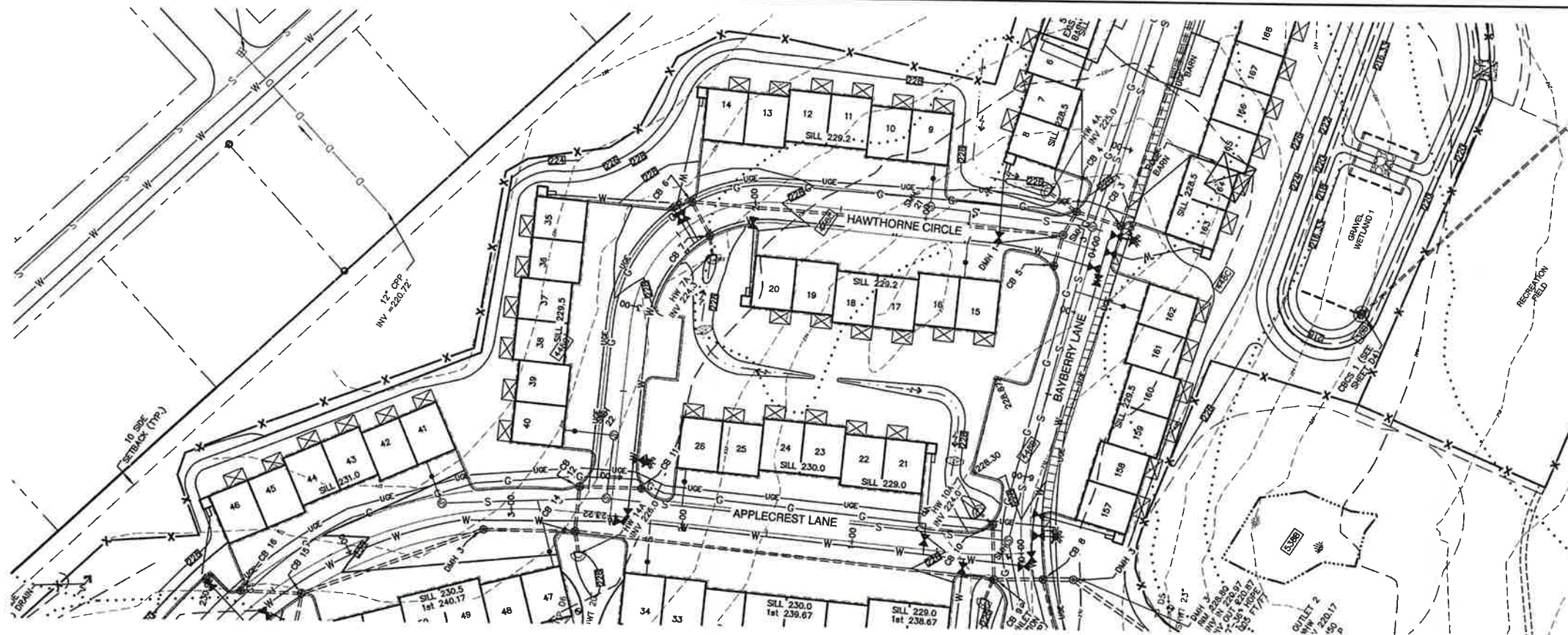
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Plan Name:	PLAN AND PROFILE
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
P4
 SHEET 27 OF 48
 JBE PROJECT NO. 21090



GRAPHIC SCALE
(IN FEET)
1 inch = 40 ft Horiz
1 inch = 4 ft Vert

HAWTHORNE CIRCLE

Design: JAC Draft: LAZ Date: 04/29/21
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Plan Name: **PLAN AND PROFILE**
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DRAWING No.

P5

SHEET 28 OF 48
JBE PROJECT NO. 21090



Design: JAC
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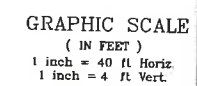
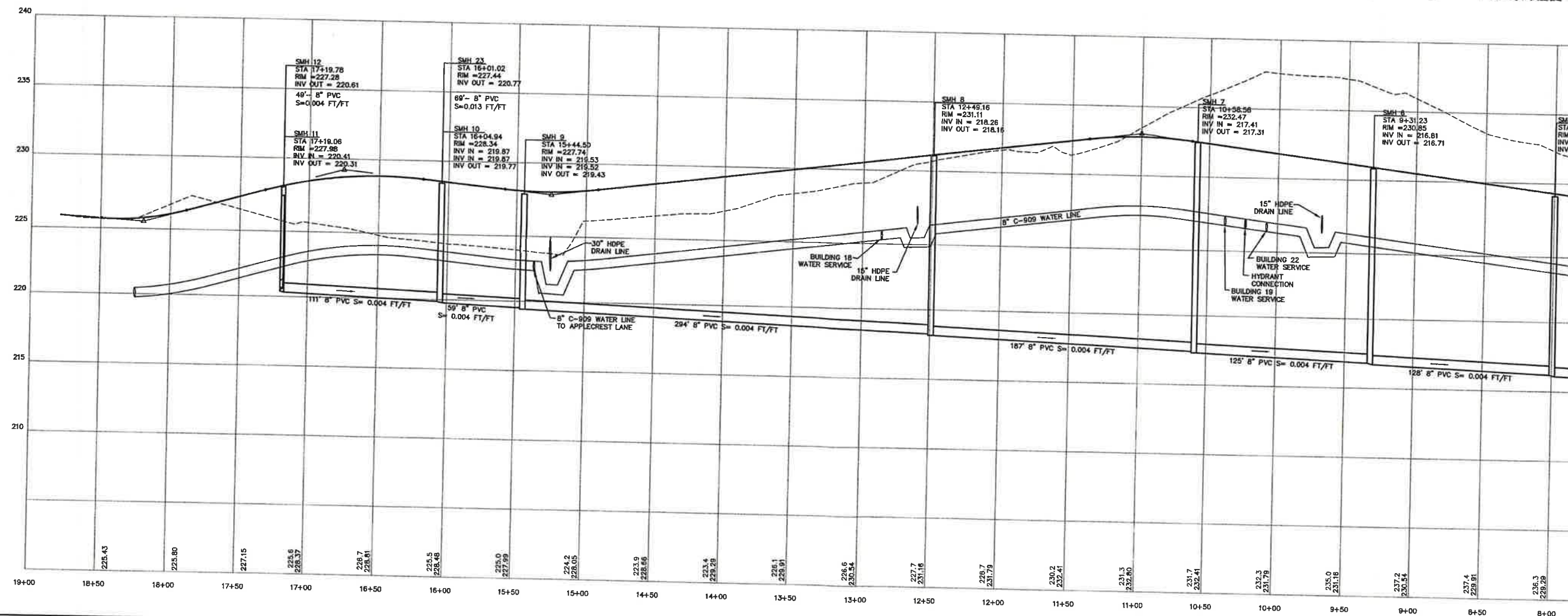
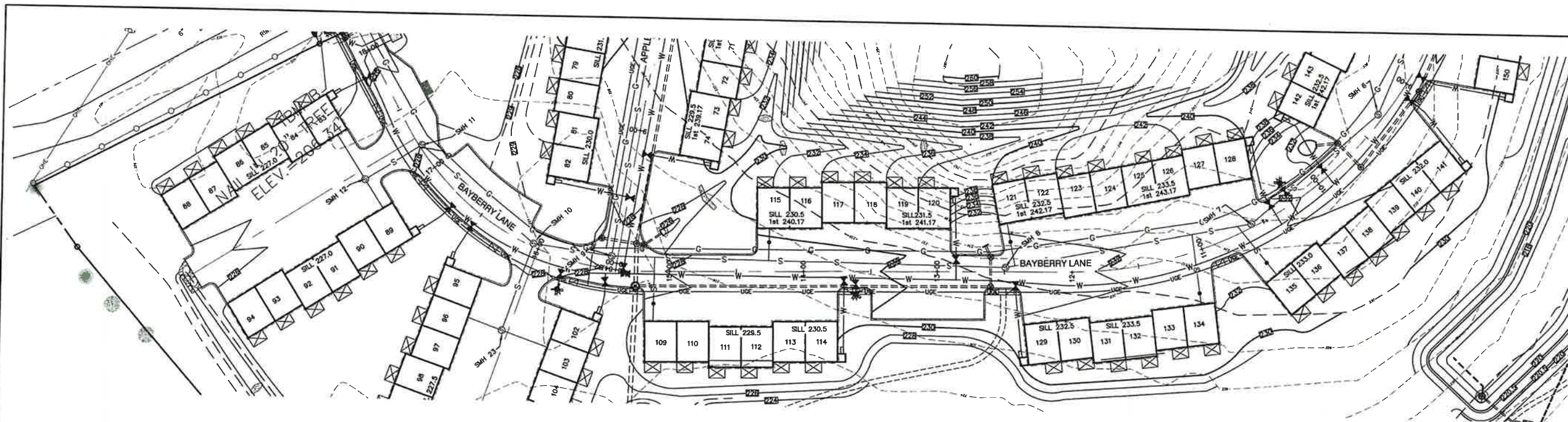


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Plan Name: **SEWER PLAN AND PROFILE**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
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DRAWING No.
P6
SHEET 29 OF 48
JBE PROJECT NO. 21090



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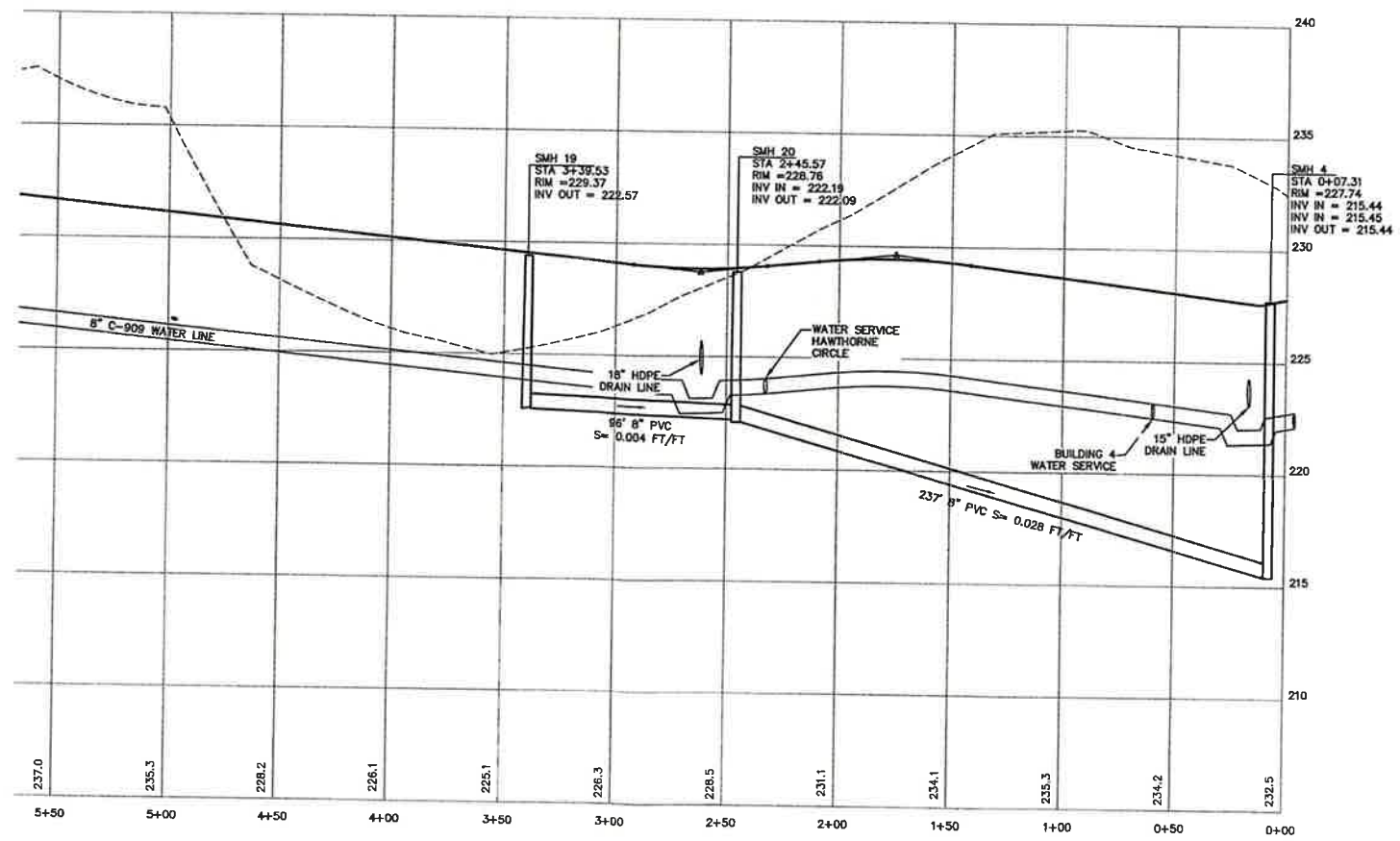
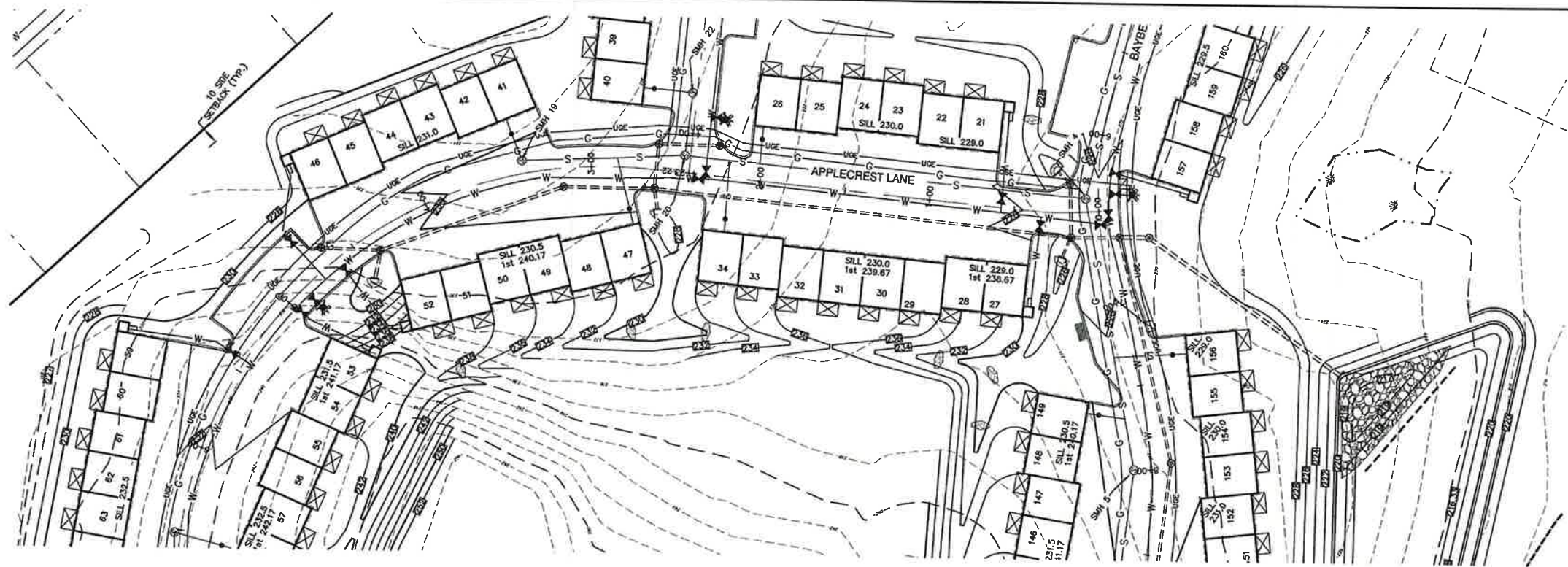


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Plan Name:	SEWER PLAN AND PROFILE
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DRAWING No.
P7
 SHEET 30 OF 48
 JBE PROJECT NO 21090



APPLECREST LANE

GRAPHIC SCALE
(IN FEET)
1 inch = 40 ft Horiz
1 inch = 4 ft Vert

Design: JAC Draft: LAZ Date: 04/29/21
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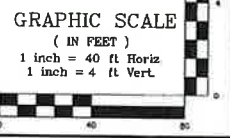
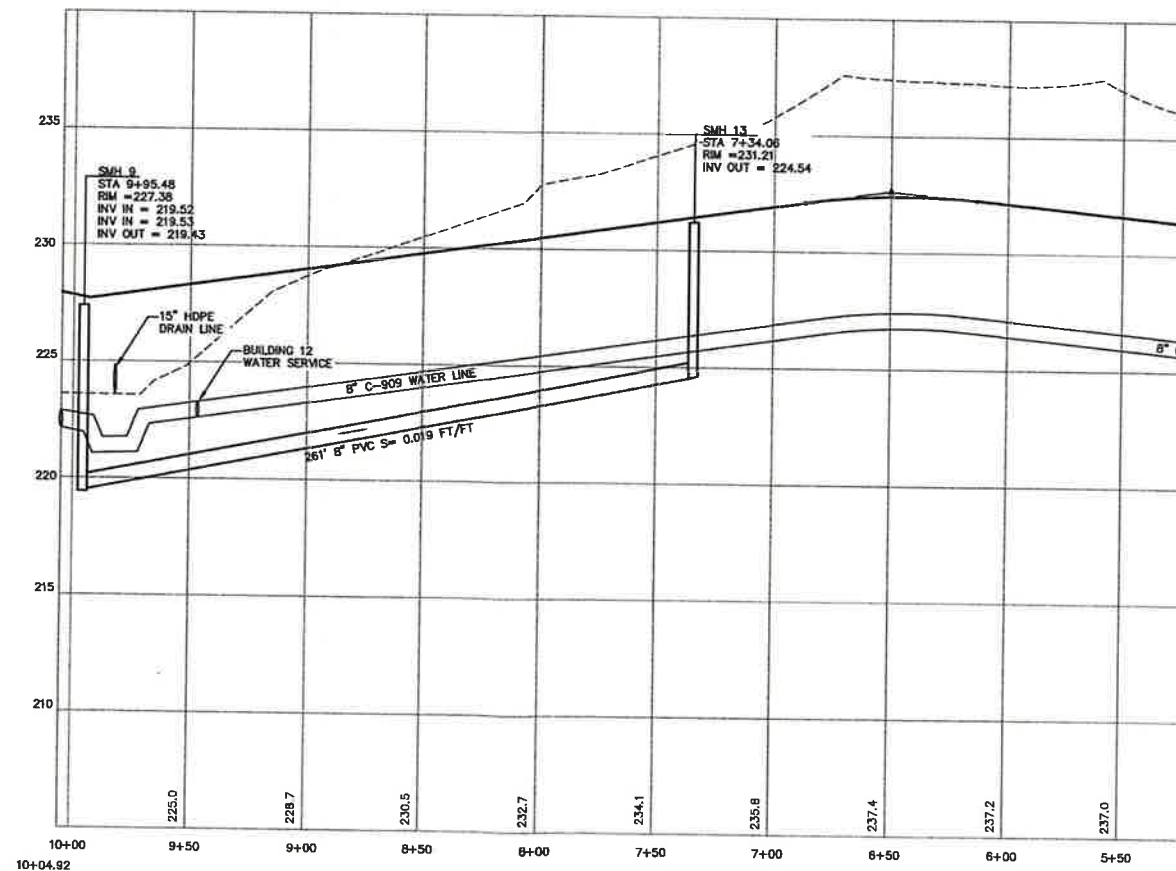
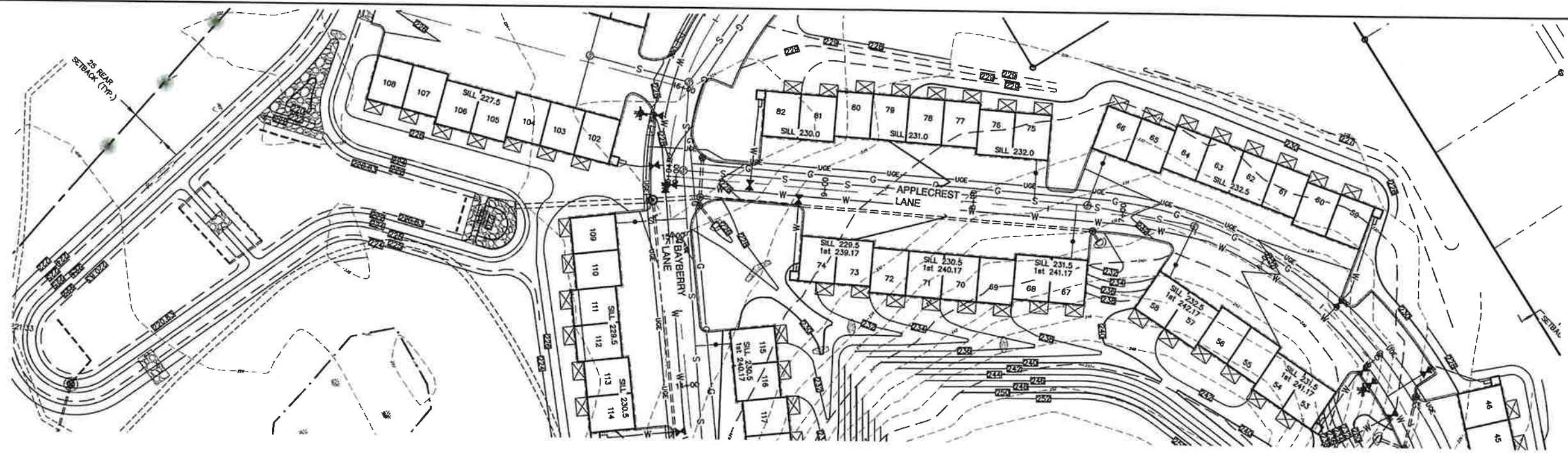


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Plan Name: **SEWER PLAN AND PROFILE**
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DRAWING No.
P8
SHEET 31 OF 46
JBE PROJECT NO. 21090



APPLECREST LANE

Design: JAC
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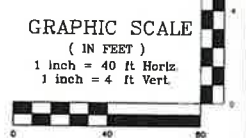
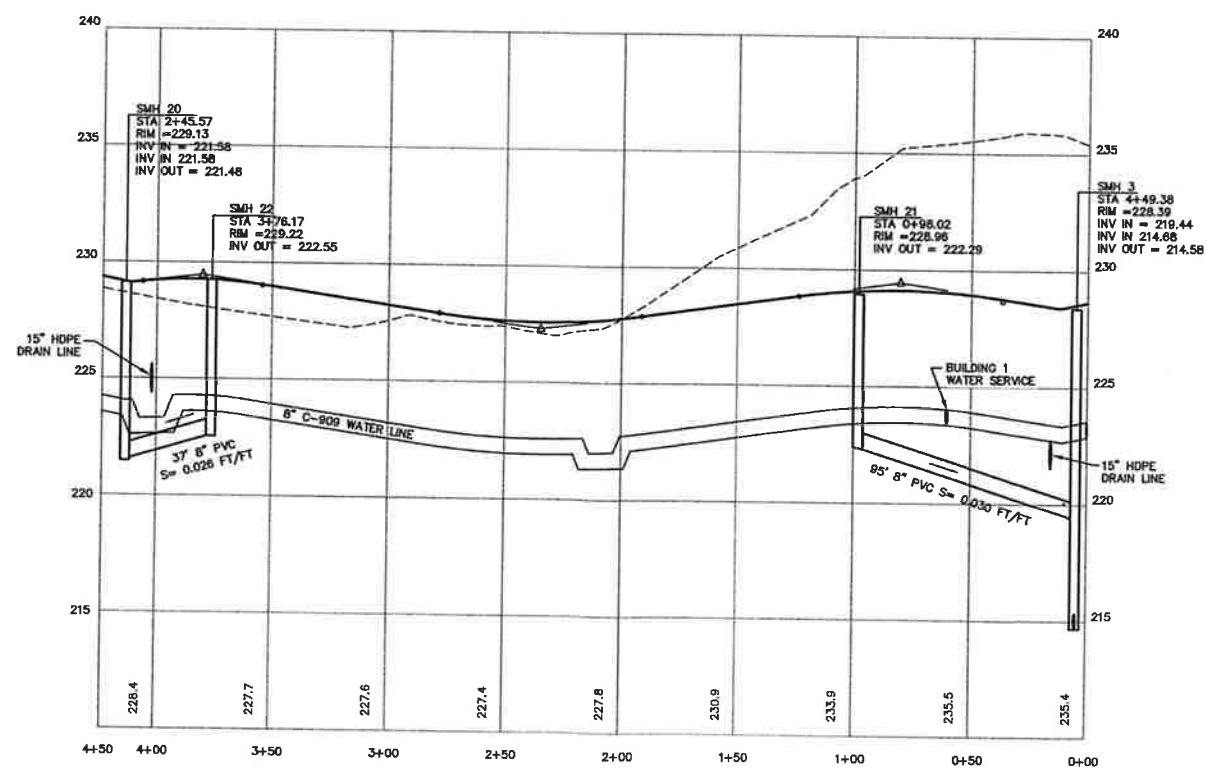
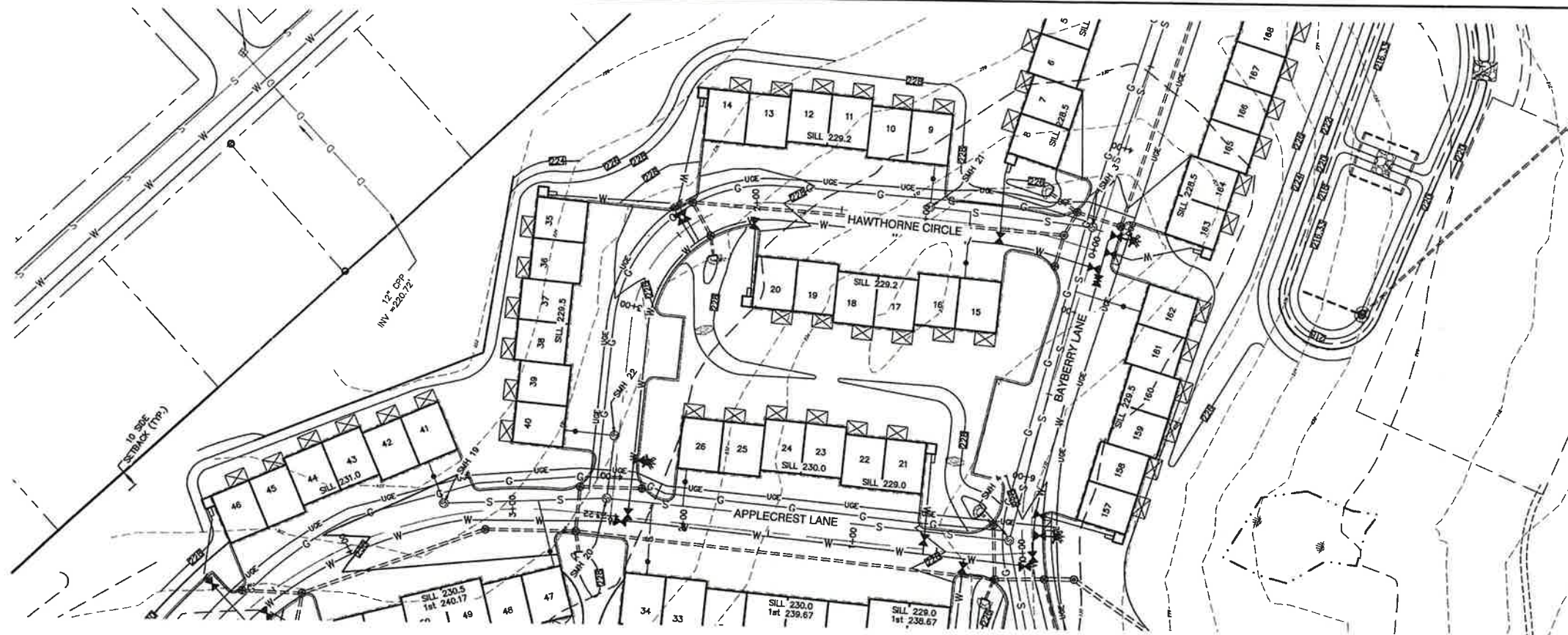


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DRAWING No.
P9
 SHEET 32 OF 48
 JBE PROJECT NO. 21090



HAWTHORNE CIRCLE

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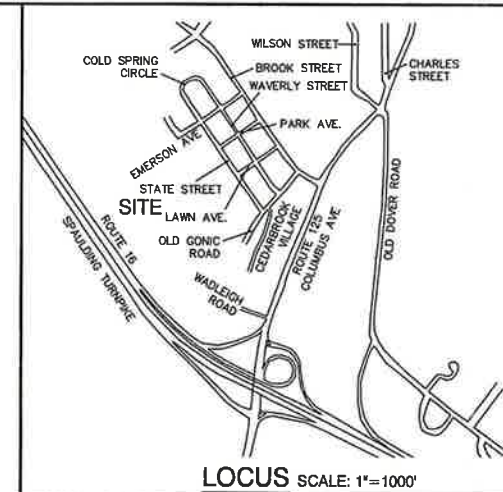


REV.	DATE	REVISION	BY
10	8/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
 Civil Engineering Services
 85 Portsmouth Ave.
 PO Box 219
 Stratham, NH 03885
 603-772-4746
 FAX: 603-772-0227
 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	SEWER PLAN AND PROFILE
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4083 PG 148

DRAWING No.
P10
 SHEET 33 OF 46
 JBE PROJECT NO. 21090



SOIL NOTES:

THIS MAP PRODUCT IS WITHIN THE TECHNICAL STANDARDS OF THE NATIONAL COOPERATIVE SOIL SURVEY. IT IS A SPECIAL PURPOSE PRODUCT, INTENDED FOR INFILTRATION REQUIREMENTS BY THE NH DES ALTERATION OF TERRAIN BUREAU. IT WAS PRODUCED BY A PROFESSIONAL SOIL SCIENTIST, AND IS NOT A PRODUCT OF THE USDA NATURAL RESOURCES CONSERVATION SERVICE. THERE IS A REPORT THAT ACCOMPANIES THIS MAP. THE SITE SPECIFIC SOIL SURVEY (SSSS) WAS PRODUCED JULY 21, 2021, AND WAS PREPARED BY JAMES P. GOVE, (SSS # 004, GOVE ENVIRONMENTAL SERVICES, INC. THE SURVEY AREA IS LOCATED AT 19 OLD GONIC ROAD, ROCHESTER, NH. SOILS WERE IDENTIFIED WITH THE NEW HAMPSHIRE STATE-WIDE NUMERICAL SOILS LEGEND, USDA NRCS, DURHAM, NH. ISSUE # 10, JANUARY 2011. THE NUMERIC LEGEND WAS AMENDED TO IDENTIFY THE CORRECT SOIL COMPONENTS OF THE COMPLEX. HYDROLOGIC SOIL GROUP FROM KSAT VALUES FOR NEW HAMPSHIRE SOILS, SOCIETY OF SOIL SCIENTISTS OF NEW ENGLAND, SPECIAL PUBLICATION NO. 5, SEPTEMBER, 2009.

SSSS SYM	SSSS MAP NAME	HISS SYM	HYDRO. SOIL GRP.
953	BOXFORD (SOMEWHAT POORLY DRAINED)	453	C
38	ELDRIDGE LOAMY SAND	343	C
141	HOLLIS-ROCK OUTCROP-CHATFIELD	228	D
448	SITUATE FINE SANDY LOAM	325	C
5	ODDORTHERENTS, LOAMY	762	N/A
538	SQUAMSCOTT LOAMY SAND	543	C

SLOPE PHASE:

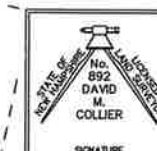
0-8% B 8-15% C 15-25% D
25%+ E

CERTIFICATION:

I CERTIFY THAT THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION, THAT IT IS THE RESULT OF A FIELD SURVEY BY THIS OFFICE AND HAS AN UNADJUSTED LINEAR ERROR OF CLOSURE THAT EXCEEDS BOTH THE MINIMUM OF 1:10,000 AS DEFINED IN SECTION 503.04 OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES AND THE MINIMUM OF 1:15,000 AS DEFINED IN SECTION 4.2 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

THIS SURVEY CONFORMS TO A CATEGORY 1 CONDITION 1 SURVEY AS DEFINED IN SECTION 4.1 OF THE N.H.L.S.A. ETHICS AND STANDARDS.

I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.



DAVID M. COLLIER, LLS 892
ON BEHALF OF JONES & BEACH ENGINEERS, INC.

DATE:

PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,895 SQ. FT. ±
30.07 ACRES ±

GRAPHIC SCALE

(IN FEET)
1 inch = 40 ft

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg
THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM JONES & BEACH ENGINEERS, INC. (JBE). ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO JBE.

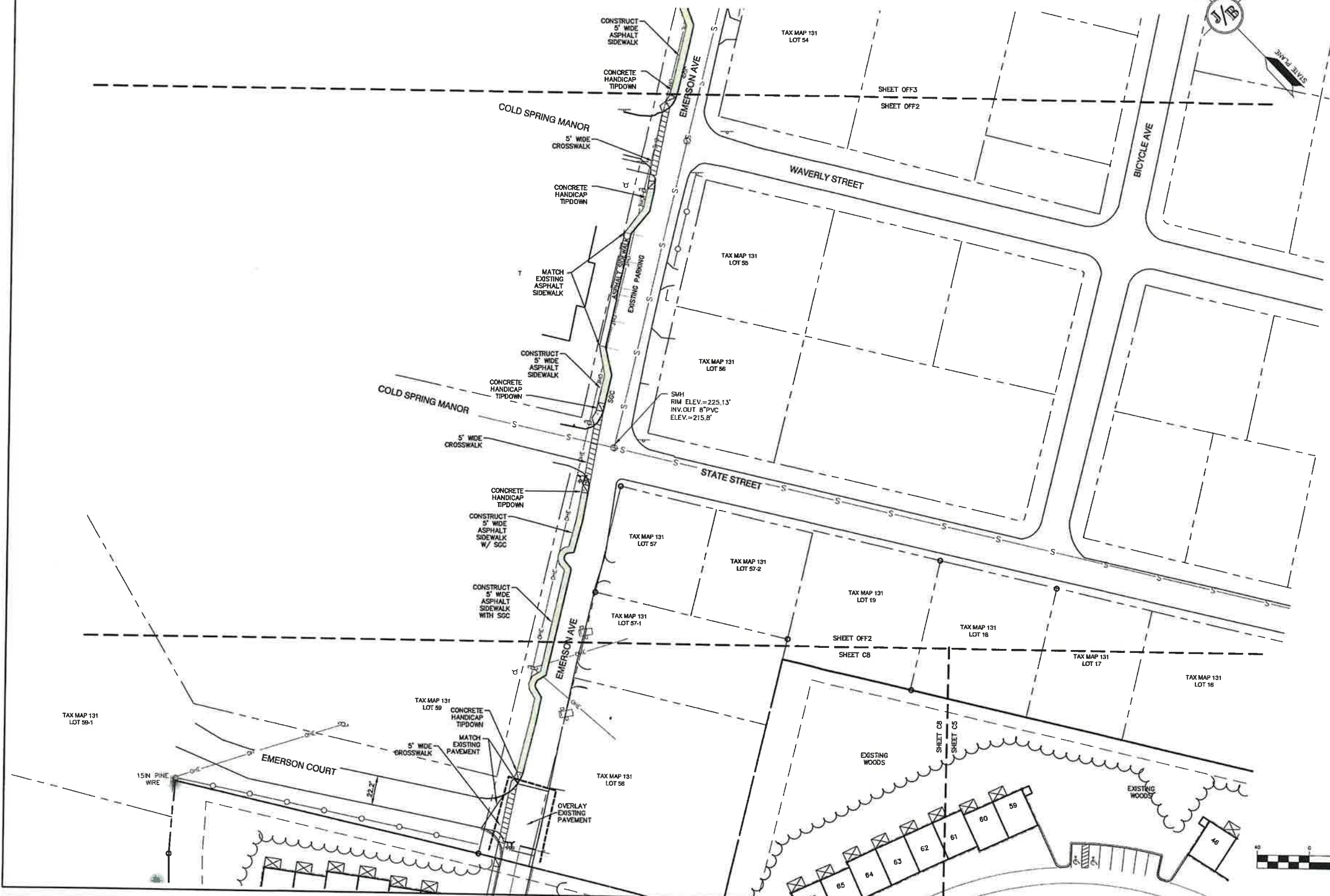
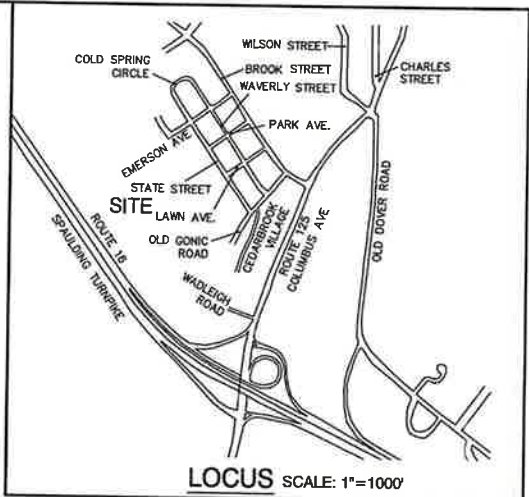


REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/28/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
Designed and Produced in NH
65 Portsmouth Ave. Civil Engineering Services 603-772-4746
PO Box 219 Stratham, NH 03885 FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **OFFSITE EXISTING CONDITIONS PLAN**
Project: **BAYBERRY COMMONS
19 OLD GONIC ROAD, ROCHESTER, NH**
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148**

DRAWING No.
OFF1
SHEET 34 OF 46
JBE PROJECT NO. 21090



PROJECT PARCEL CITY OF ROCHESTER TAX MAP 131, LOT 10
APPLICANT GREEN AND COMPANY 11 LAFAYETTE ROAD NORTH HAMPTON, NH 03862
TOTAL LOT AREA 1,309,695 SQ. FT. ± 30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg
THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM JONES & BEACH ENGINEERS, INC. (JBE). ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO JBE.

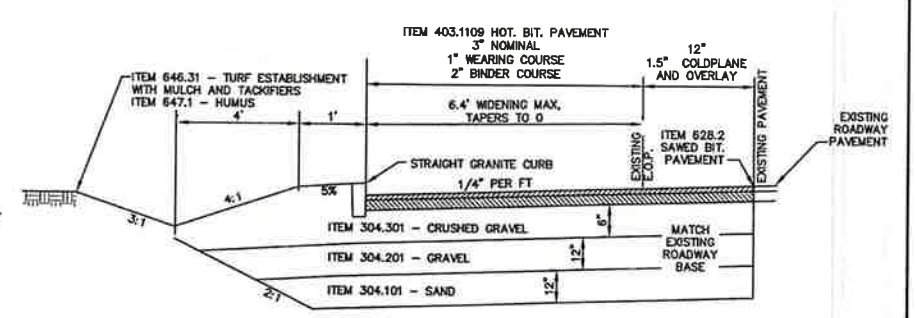
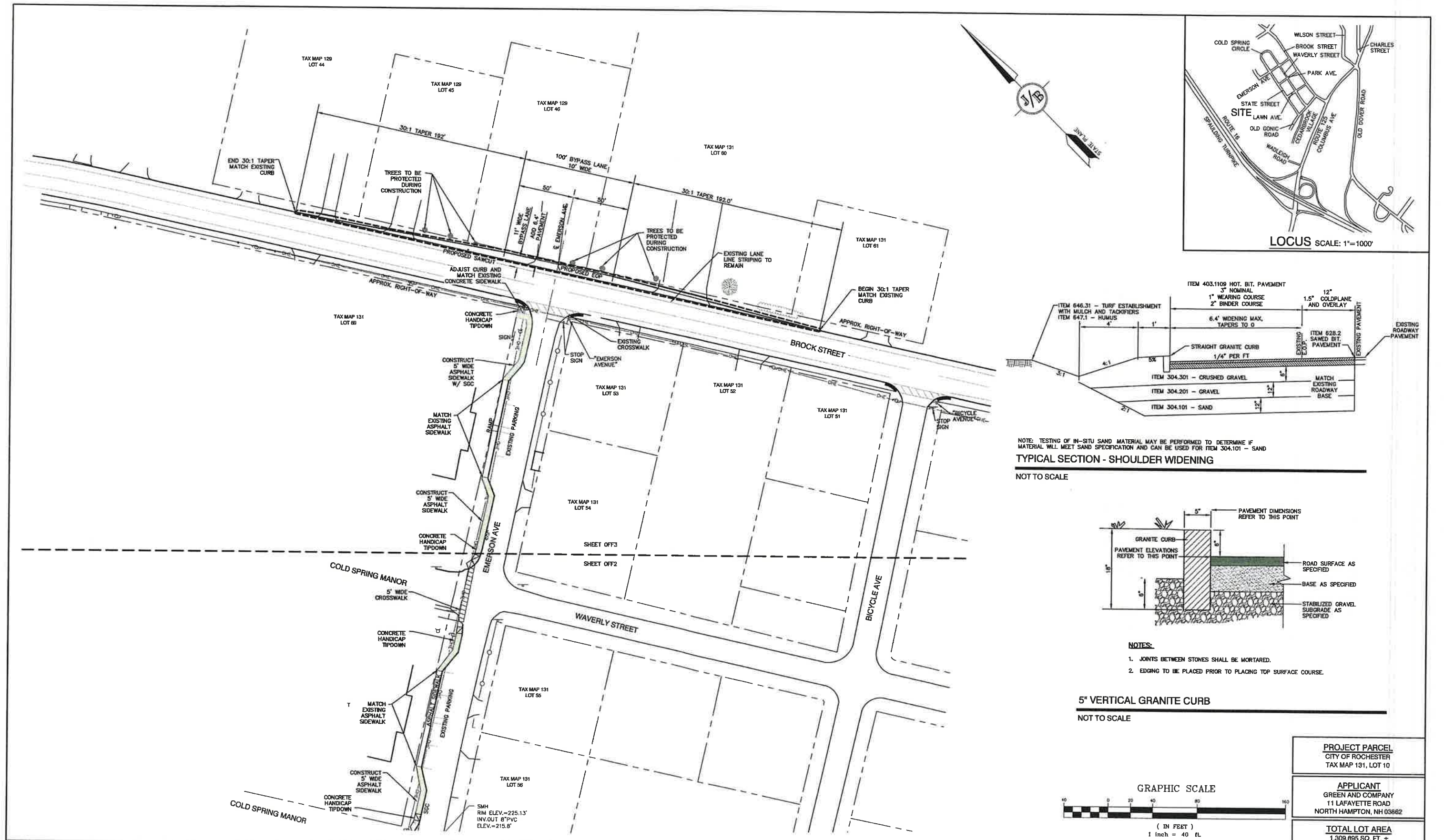


REV.	DATE	REVISION	BY
10	8/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/18/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

J/B Jones & Beach Engineers, Inc.
Civil Engineering Services
85 Portsmouth Ave.
PO Box 219
Stratham, NH 03885
603-772-4748
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	OFFSITE SITE PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

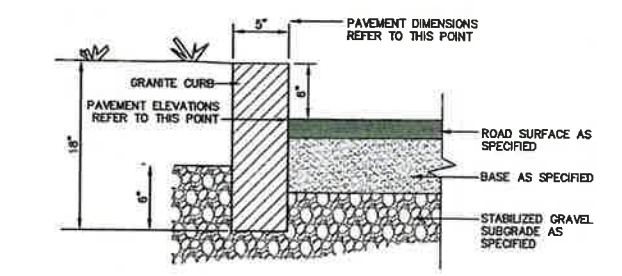
DRAWING No.
OFF2
SHEET 35 OF 49
JBE PROJECT NO. 21090



NOTE: TESTING OF IN-SITU SAND MATERIAL MAY BE PERFORMED TO DETERMINE IF MATERIAL WILL MEET SAND SPECIFICATION AND CAN BE USED FOR ITEM 304.101 - SAND

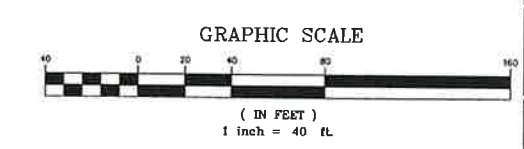
TYPICAL SECTION - SHOULDER WIDENING

NOT TO SCALE



- NOTES:**
1. JOINTS BETWEEN STONES SHALL BE MORTARED.
 2. EDGING TO BE PLACED PRIOR TO PLACING TOP SURFACE COURSE.

5\"/>



PROJECT PARCEL CITY OF ROCHESTER TAX MAP 131, LOT 10
APPLICANT GREEN AND COMPANY 11 LAFAYETTE ROAD NORTH HAMPTON, NH 03862
TOTAL LOT AREA 1,309,895 SQ. FT. ± 30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	9/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	9/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	8/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

Designed and Produced in NH

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PO Box 219
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Civil Engineering Services

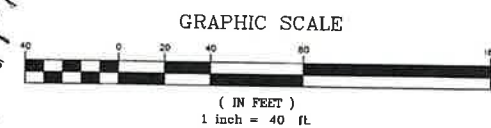
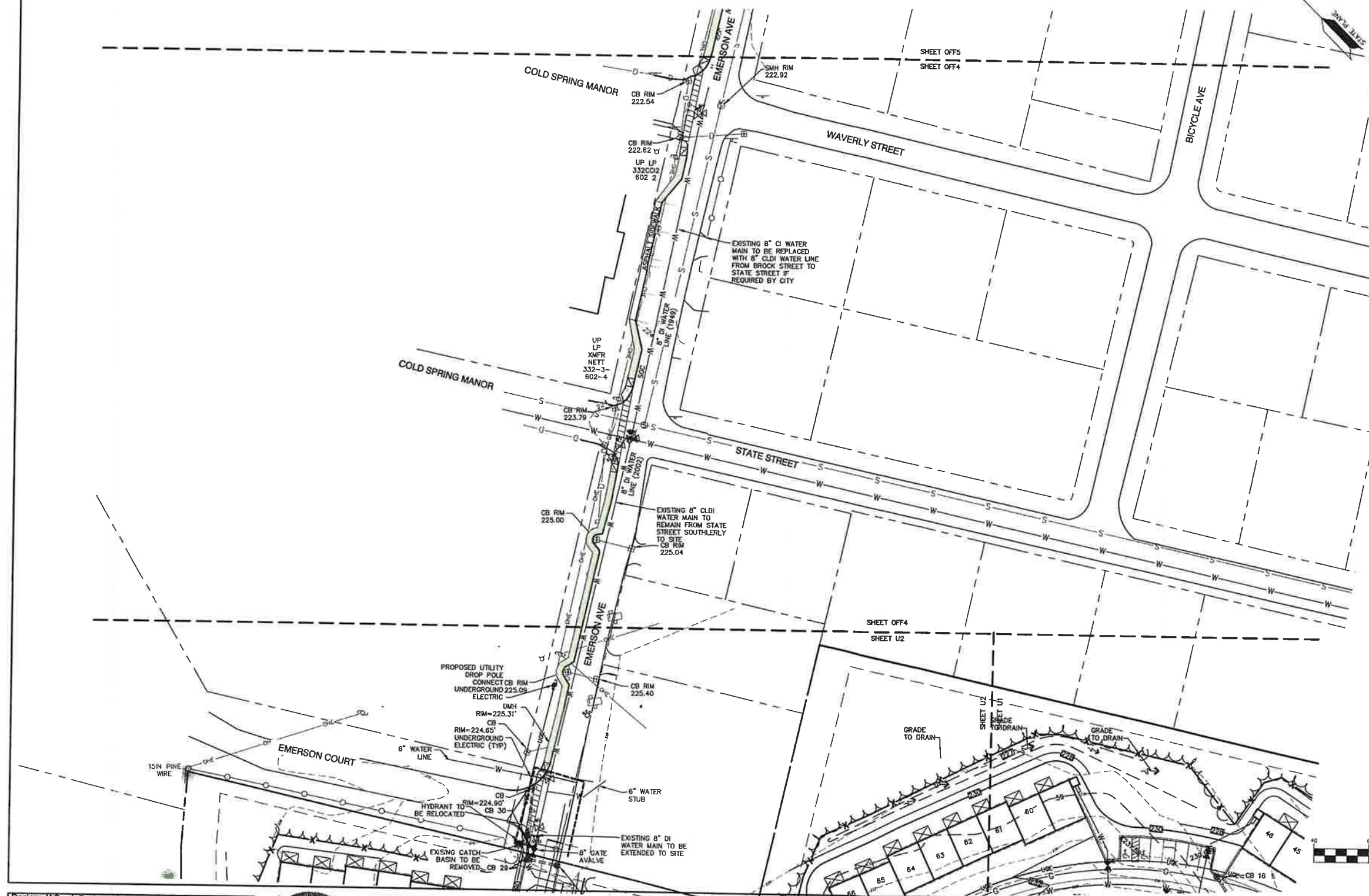
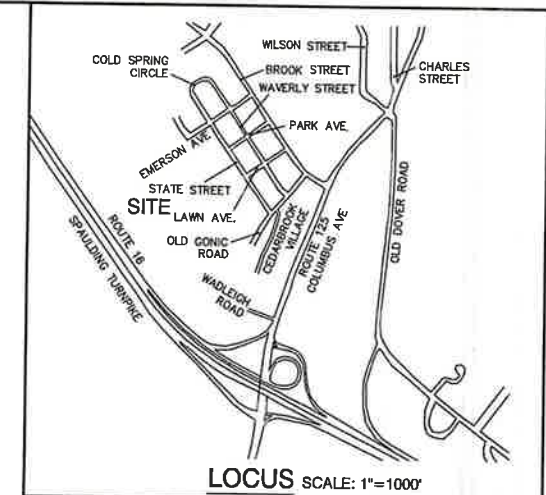
603-772-4746
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	OFFSITE SITE PLAN
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.

OFF3

SHEET 36 OF 48
JBE PROJECT NO. 21090



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg
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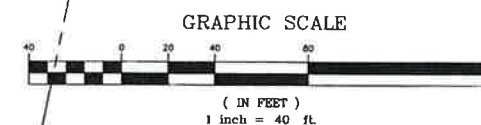
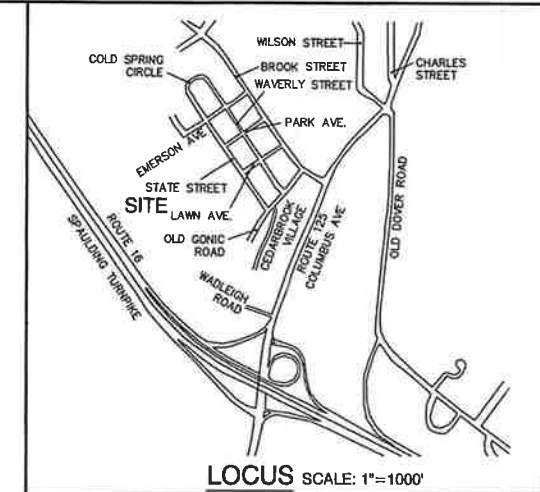
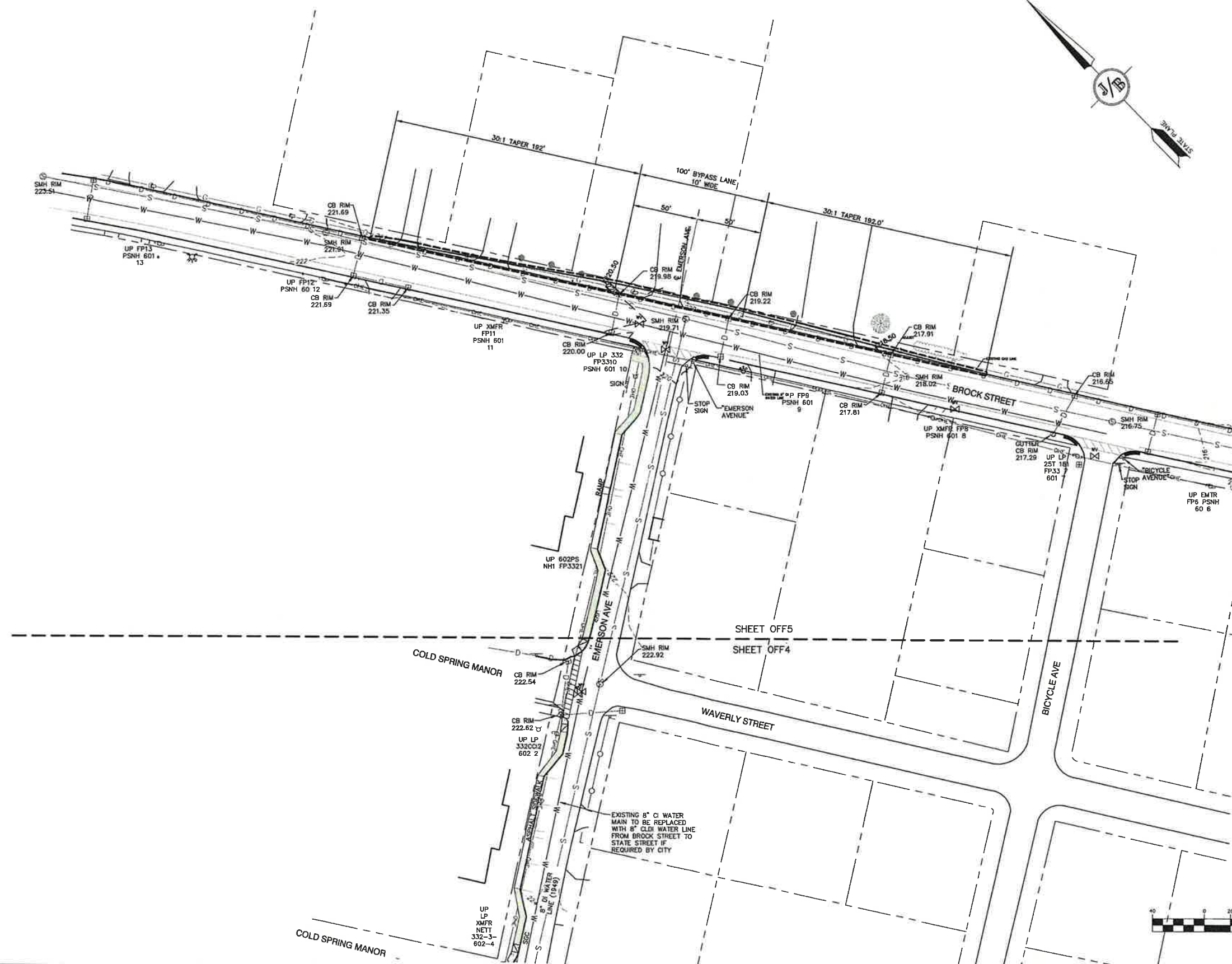


REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	6/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/18/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

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Civil Engineering Services
85 Portsmouth Ave.
PO Box 219
Stratham, NH 03885
Designed and Produced in NH
603-772-4748
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **OFFSITE GRADING AND UTILITY PLAN**
Project: **BAYBERRY COMMONS**
19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE**
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.
OFF4
SHEET 37 OF 46
JBE PROJECT NO. 21090



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg
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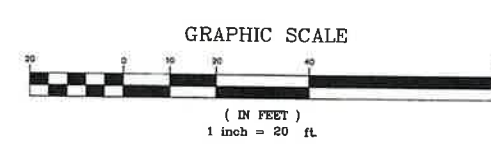
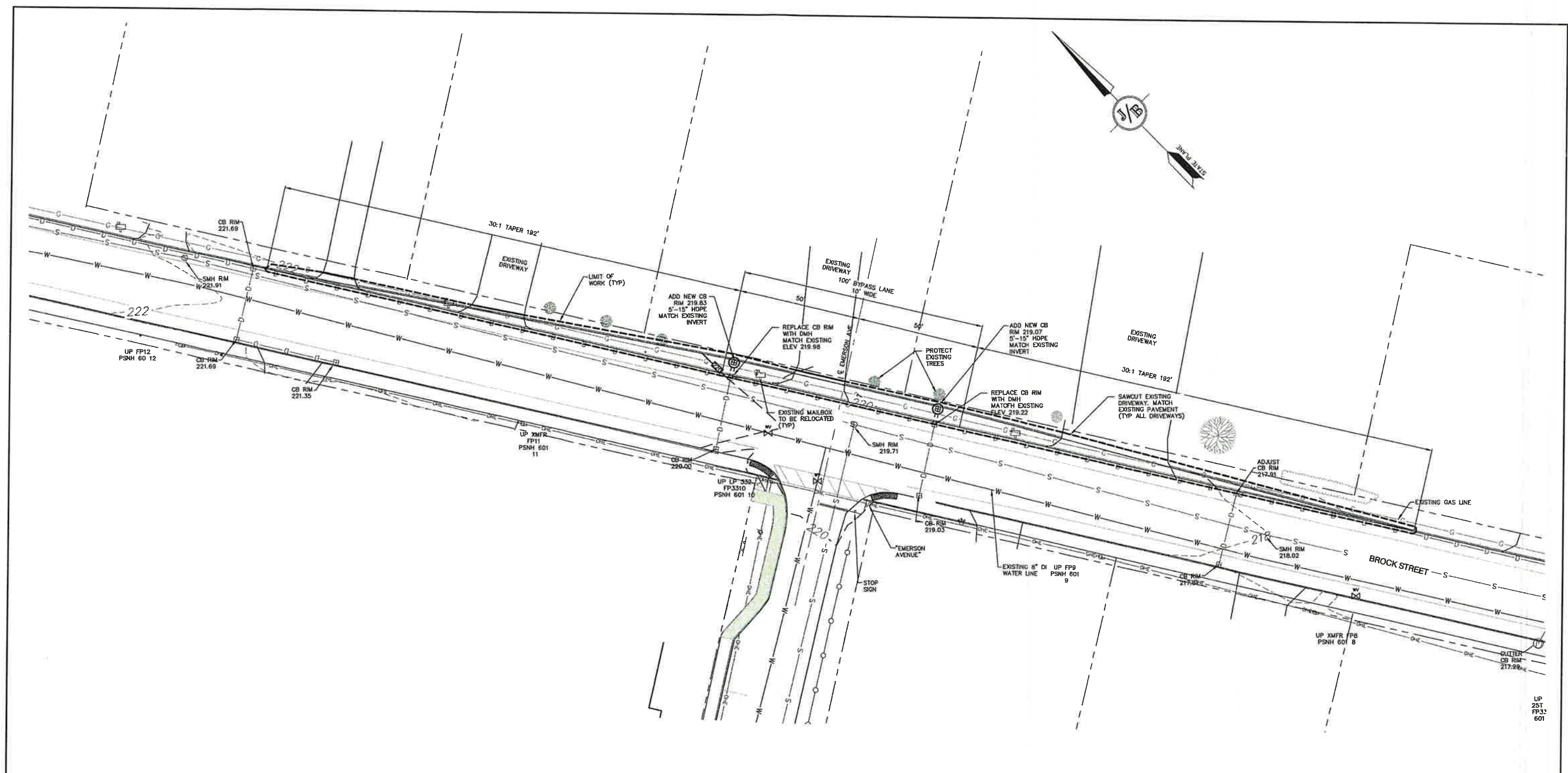


10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ
REV.	DATE	REVISION	BY

Designed and Produced in NH
J/B Jones & Beach Engineers, Inc.
65 Portsmouth Ave. Civil Engineering Services 603-772-4746
PO Box 219 FAX: 603-772-0227
Stratham, NH 03886 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name: **OFFSITE GRADING AND UTILITY PLAN**
Project: **BAYBERRY COMMONS
19 OLD GONIC ROAD, ROCHESTER, NH**
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148**

DRAWING No.
OFF5
SHEET 38 OF 46
JBE PROJECT NO. 21090



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES AGT COMMENTS	LAZ
9	6/28/22	REVISED PER NHDES AGT COMMENTS	LAZ
8	6/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/28/22	REVISED PER TRAFFIC COMMENTS	LAZ

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Civil Engineering Services

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Stratham, NH 03885

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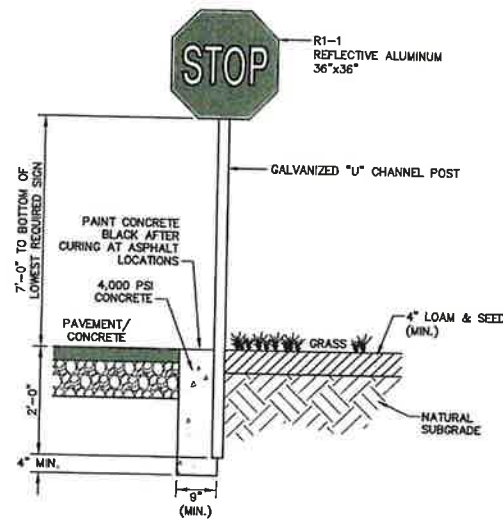
Plan Name: **OFFSITE GRADING AND UTILITY PLAN**

Project: **BAYBERRY COMMONS
19 OLD GONIC ROAD, ROCHESTER, NH**

Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148**

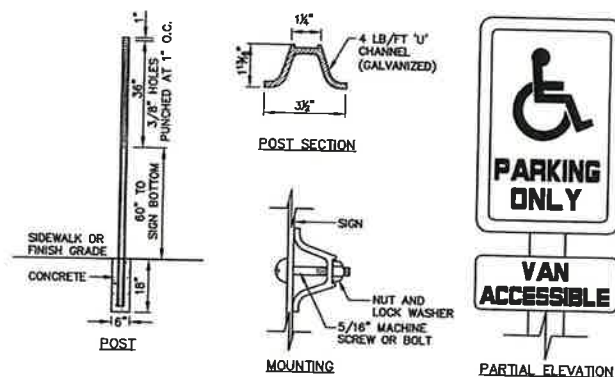
DRAWING No.
OFF6

SHEET 39 OF 46
JBE PROJECT NO. 21090



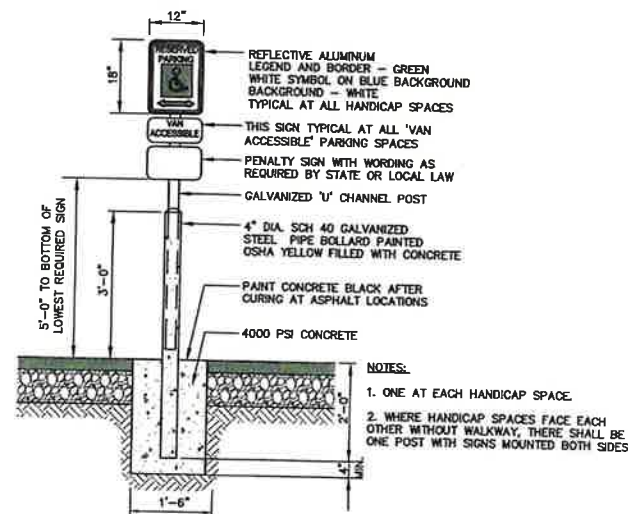
STOP SIGN (R1-1)

NOT TO SCALE



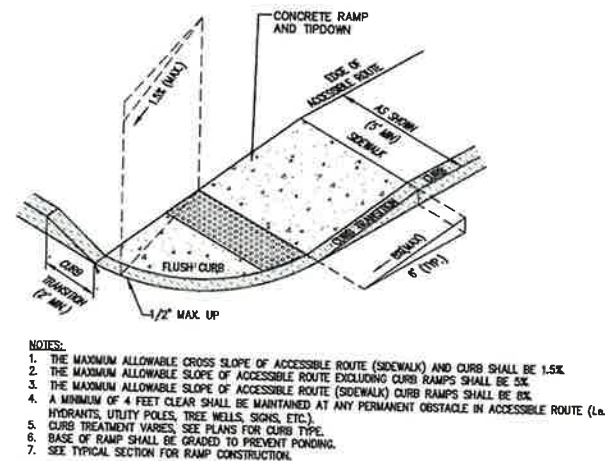
HANDICAP SIGN DETAILS

NOT TO SCALE



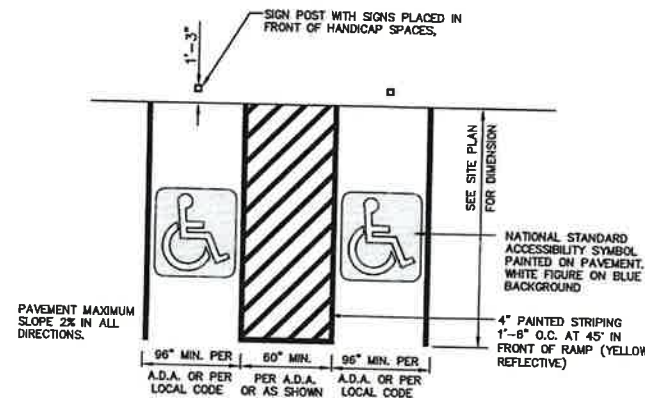
HANDICAP PARKING SIGN (R7-8)

NOT TO SCALE



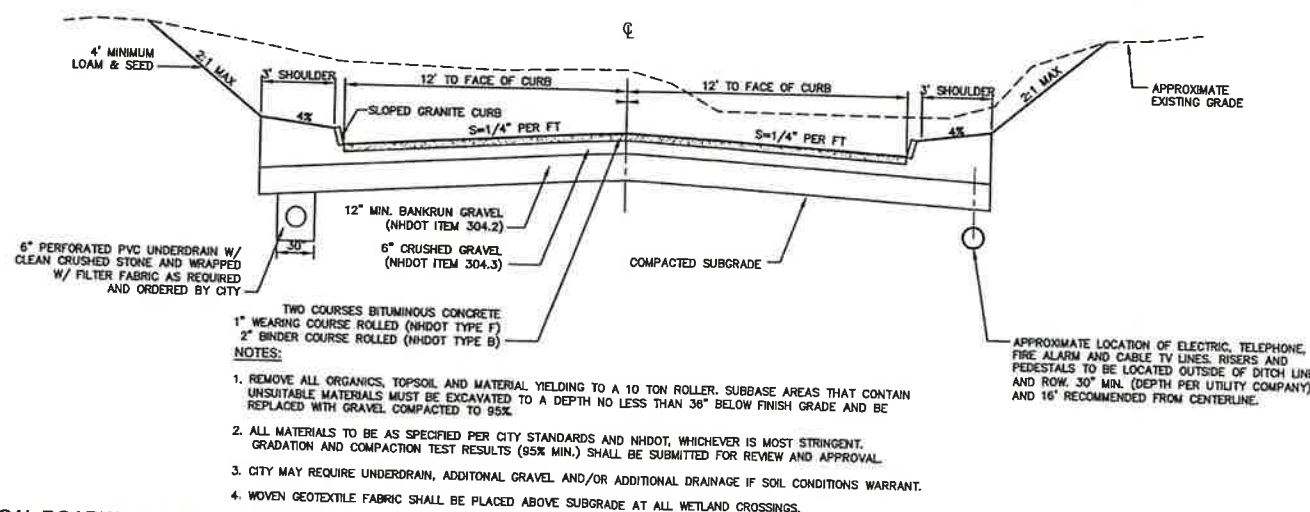
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NOT TO SCALE



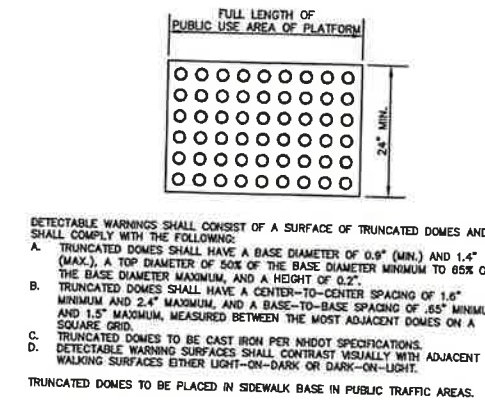
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NOT TO SCALE

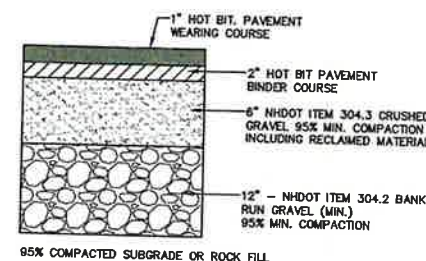


TYPICAL ROADWAY SECTION W/CURBING

NOT TO SCALE

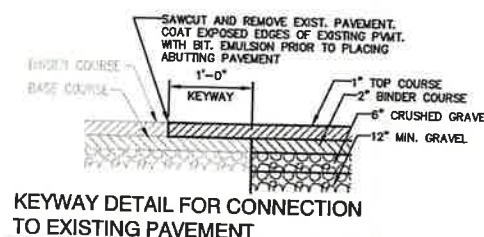


ACCESSIBLE CURB RAMP TRUNCATED DOMES



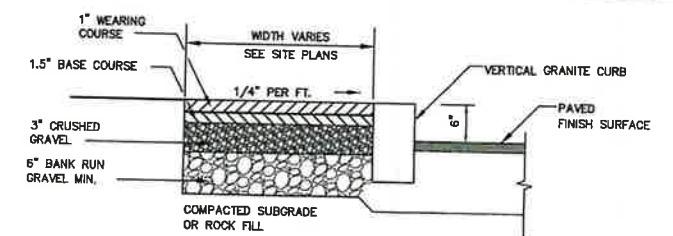
TYPICAL BITUMINOUS PAVEMENT

NOT TO SCALE



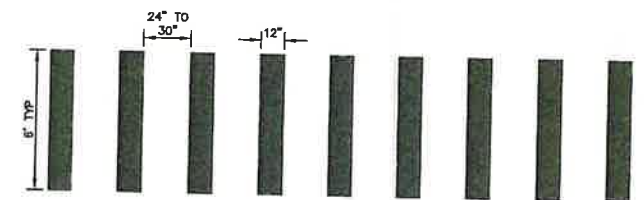
KEYWAY DETAIL FOR CONNECTION TO EXISTING PAVEMENT

NOT TO SCALE



BIT. SIDEWALK W/ VERTICAL GRANITE CURB

NOT TO SCALE

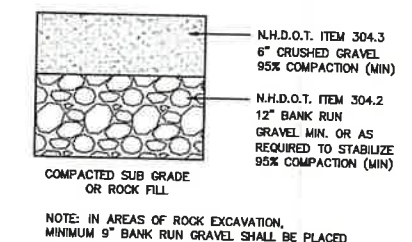


NOTES:

1. TRANSVERSE CROSSWALK LINES SHALL BE THERMOPLASTIC, NOT LESS THAN 6" WIDE AND NOT LESS THAN 6" APART.
2. SPACING FOR THE CONTINENTAL BLOCK MARKINGS SHALL BE UNIFORM FOR EACH INDIVIDUAL CROSSWALK BUT CAN BE MODIFIED FOR ONE CROSSWALK TO THE NEXT TO ELIMINATE A CROSSWALK MARKING DIRECTLY IN THE WHEELPATH.

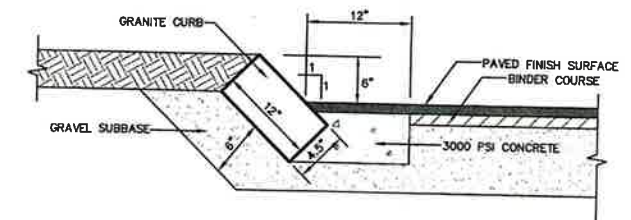
NHDOT CONTINENTAL BLOCK MARKING DETAIL

NOT TO SCALE



GRAVEL SECTION

NOT TO SCALE



NOTES:

1. CURB TO BE PLACED PRIOR TO PLACING TOP SURFACE COURSE.
2. JOINTS BETWEEN STONES SHALL BE MORTARED.

SLOPED GRANITE CURB

NOT TO SCALE

Design: JAC	Draft: LAZ	Date: 04/29/21
Checked: JAC	Scale: AS NOTED	Project No.: 21090
Drawing Name: 21090-PLAN.dwg		
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REV.	DATE	REVISION	BY
10	8/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

Designed and Produced in NH

J/B Jones & Beach Engineers, Inc.

85 Portsmouth Ave.
PO Box 219
Stratham, NH 03885

Civil Engineering Services

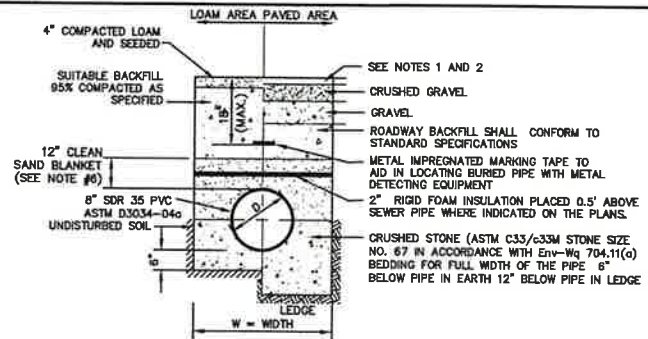
603-772-4746
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	DETAIL SHEET
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.

D1

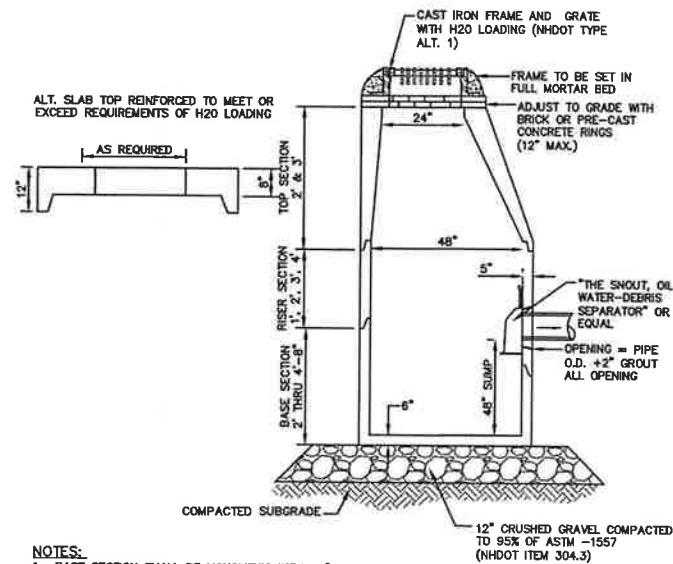
SHEET 40 OF 46
JBE PROJECT NO. 21090



- NOTES:
1. PAVEMENT REPAIR IN EXISTING ROADWAYS SHALL CONFORM TO PAVEMENT DETAILS.
 2. NEW ROADWAY CONSTRUCTION SHALL CONFORM TO SUBDIVISION SPECIFICATIONS.
 3. TRENCH BACKFILL SHALL CONFORM WITH ENV. WQ 704.11(h) AND BE FREE OF DEBRIS, PAVEMENT, ORGANIC MATTER, TOP SOIL, WET OR SOFT MUCK, PEAT OR CLAY, EXCAVATED LEDGE OR ROCKS OVER SIX INCHES.
 4. W= MAXIMUM ALLOWABLE TRENCH WIDTH TO A PLANE 12" INCHES ABOVE THE PIPE. FOR PIPES 15 INCHES NOMINAL DIAMETER OR LESS, WIDTH SHALL BE NO MORE THAN 36"; FOR PIPES GREATER THAN 15 INCHES NOMINAL DIAMETER, WIDTH SHALL BE 24 INCHES PLUS PIPE O.D. WIDTH SHALL ALSO BE THE PAYMENT WIDTH FOR LEDGE EXCAVATION AND FOR ORDERED EXCAVATION BELOW GRADE.
 5. RIGID FOAM INSULATION TO BE PROVIDED WHERE COVER IN THE ROADWAY IS LESS THAN 6" AND CROSS COUNTRY IS LESS THAN 4", PURSUANT TO DES WAIVER BEING ISSUED.
 6. PIPE SAND BLANKET MATERIAL SHALL BE GRADED SAND, FREE FROM ORGANIC MATERIALS, GRADED SUCH THAT 100% PASSES A 1/2" SIEVE AND A MAXIMUM OF 15% PASSES A #200 SIEVE IN ACCORDANCE WITH ENV-WQ 704.11(b).
 7. JOINT SEALS FOR PVC PIPE SHALL BE OIL RESISTANT COMPRESSION RINGS OF ELASTOMERIC MATERIAL AND CERTIFIED BY THE MANUFACTURER AS CONFORMING TO THE ASTM D3212 STANDARD IN EFFECT WHEN THE JOINT SEALS WERE MANUFACTURED, AND SHALL BE PUSH-ON, BELL-AND-SPIGOT TYPE PER ENV-WQ 704.05 (c).

SEWER TRENCH

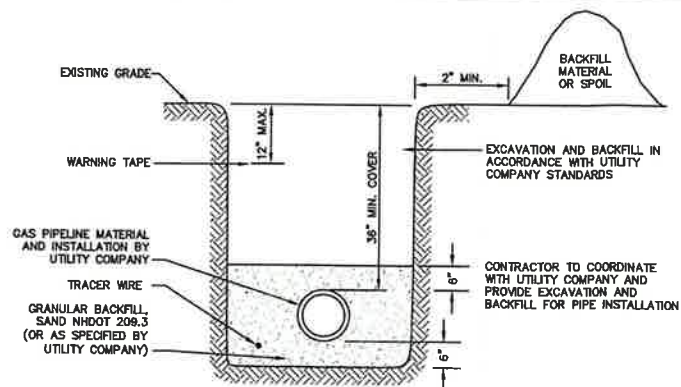
NOT TO SCALE



- NOTES:
1. BASE SECTION SHALL BE MONOLITHIC WITH 48" INSIDE DIAMETER.
 2. ALL SECTIONS SHALL BE DESIGNED FOR H2O LOADING.
 3. CONCRETE SHALL BE COMPRESSIVE STRENGTH 4000 PSI, TYPE II CEMENT.
 4. FRAMES AND GRATES SHALL BE HEAVY DUTY AND DESIGNED FOR H2O LOADING.
 5. PROVIDE "V" KNOCKOUTS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE. MORTAR ALL PIPE CONNECTIONS SO AS TO BE WATERTIGHT.
 6. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE BUTYL RUBBER.
 7. ALL CATCH BASIN FRAMES AND GRATES SHALL BE NHDOT CATCH BASIN TYPE ALTERNATE 1 OR NEEDNAH R-3570 OR APPROVED EQUAL (24"x24" TYPICAL).
 8. STANDARD CATCH BASIN FRAME AND GRATE(S) SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM, BUT NO MORE THAN 12"), OR PRECAST CONCRETE "DONUTS".
 9. ALL CATCH BASINS ARE TO BE FITTED WITH GREASE HOODS.

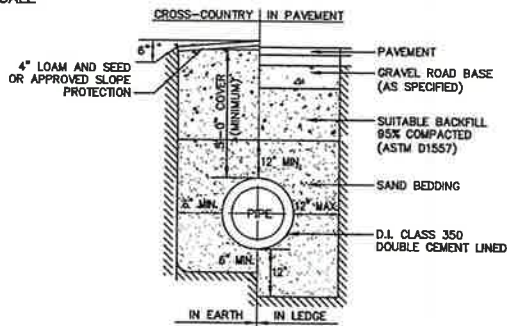
CATCH BASIN WITH GREASE HOOD

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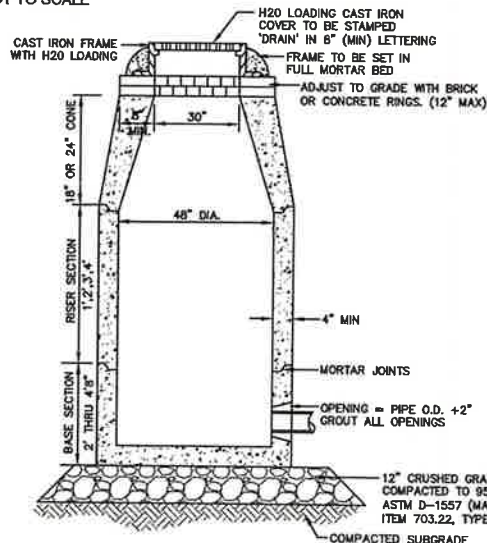
GAS TRENCH

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WATER SYSTEM TRENCH

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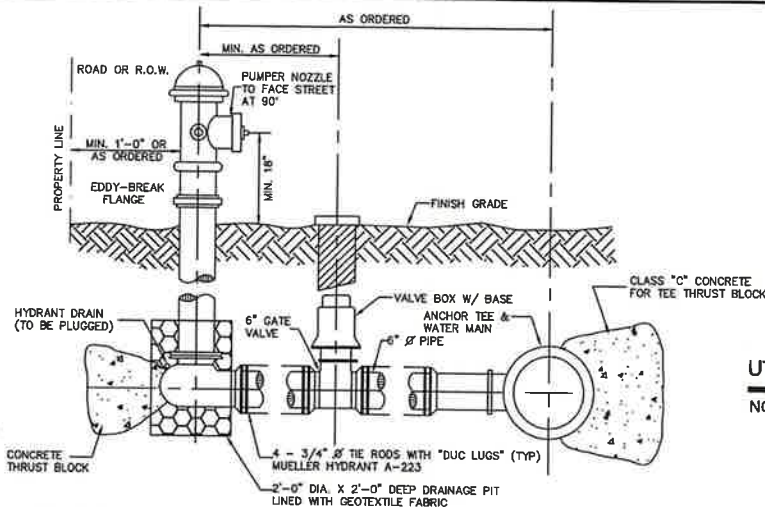


NOTES:

1. BASE SECTION SHALL BE MONOLITHIC WITH 48" INSIDE DIAMETER.
2. ALL SECTIONS SHALL BE DESIGNED FOR H2O LOADING.
3. CONCRETE SHALL BE COMPRESSIVE STRENGTH 4000 PSI, TYPE II CEMENT.
4. FRAMES AND GRATES SHALL BE HEAVY DUTY AND DESIGNED FOR H2O LOADING.
5. PROVIDE "V" KNOCKOUTS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE. MORTAR ALL PIPE CONNECTIONS SO AS TO BE WATERTIGHT.
6. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE BUTYL RUBBER.
7. ALL DRAIN MANHOLE FRAMES AND GRATES SHALL BE NEEDNAH R-1798 OR APPROVED EQUAL (30" DIA. TYPICAL).
8. STANDARD FRAME(S) AND GRATE(S) SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM, BUT NO MORE THAN 12"), OR PRECAST CONCRETE "DONUTS".

DRAIN MANHOLE (4' DIAM.)

NOT TO SCALE



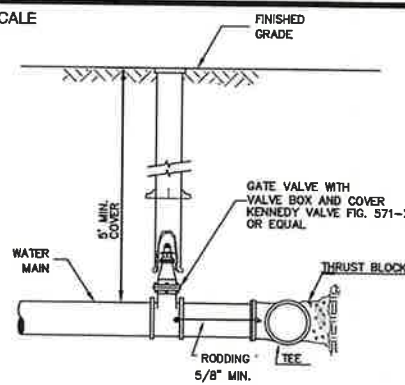
REFER TO ROCHESTER WATER DEPARTMENT SPECS.

NOTES:

1. ALL PIPE FITTINGS TO BE D.I. PRESSURE CLASS 350, THICKNESS CLASS 52.
2. HYDRANT TO BE PAINTED RED WITH WHITE "REFLECTOR" PAINT ON BONNET.
3. MECHANICAL JOINTS SHALL HAVE MEGALUG RETAINING GLANDS AS MADE BY EBBA OR APPROVED EQUAL.
4. STEAMER NOZZLE TO BE "STORCH" TYPE.
5. NATIONAL STANDARD THREAD.

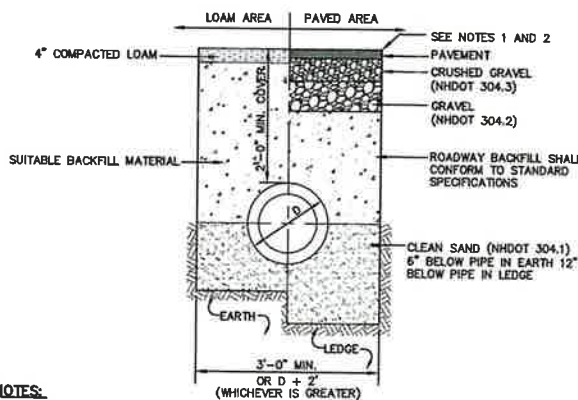
HYDRANT INSTALLATION

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BURIED GATE VALVE DETAIL

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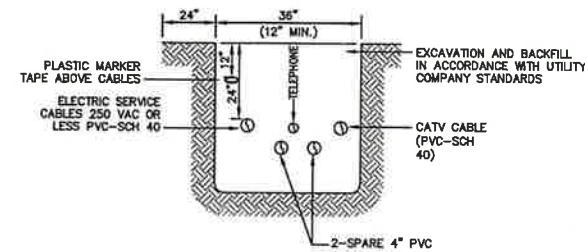


NOTES:

1. PAVEMENT REPAIR IN EXISTING ROADWAYS SHALL CONFORM TO STREET OPENING REGULATIONS.
2. NEW ROADWAY CONSTRUCTION SHALL CONFORM WITH PROJECT AND CITY SPECIFICATIONS.
3. ALL MATERIALS ARE TO BE COMPACTED TO 95% OF ASTM D-1557.

DRAINAGE TRENCH

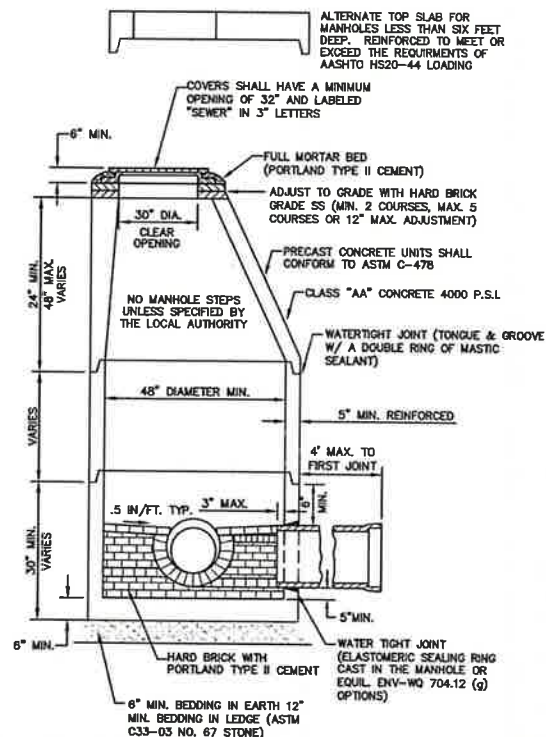
NOT TO SCALE



NOTE: ALL UTILITIES SHALL BE REVIEWED AND APPROVED BY APPROPRIATE UTILITY COMPANY.

UTILITY TRENCH

NOT TO SCALE



NOTES:

1. PER NHDES ENV-WQ 704.13(c), MORTAR USED IN MANHOLE CONSTRUCTION SHALL COMPLY WITH THE FOLLOWING:
 - a. MORTAR SHALL BE COMPOSED OF TYPE II PORTLAND CEMENT AND SAND WITH OR WITHOUT HYDRATED LIME ADDITION
 - b. PROPORTIONS IN MORTAR OF PARTS BY VOLUMES SHALL BE PER TABLE 704-4:
 - (1) 4.5 PARTS SAND AND 1.5 PARTS CEMENT; OR
 - (2) 4.5 PARTS SAND, ONE PART CEMENT AND 0.5 PART HYDRATED LIME.
 - c. CEMENT SHALL BE TYPE II PORTLAND CEMENT THAT IS CERTIFIED BY ITS MANUFACTURER AS CONFORMING TO THE ASTM C150/C150M STANDARD IN EFFECT AT THE TIME THE CEMENT WAS MANUFACTURED
 - d. HYDRATED LIME SHALL BE TYPE S THAT IS CERTIFIED BY ITS MANUFACTURER AS CONFORMING TO THE ASTM C207 STANDARD IN EFFECT AT THE TIME THE HYDRATED LIME WAS PROCESSED
 - e. SAND SHALL CONSIST OF INERT NATURAL SAND THAT IS CERTIFIED BY ITS SUPPLIER AS CONFORMING TO THE ASTM C33 STANDARD IN EFFECT AT THE TIME THE SAND IS PROCESSED BY STANDARD SPECIFICATIONS FOR CONCRETE, FINE AGGREGATES
 - f. CONCRETE FOR DROP SUPPORTS SHALL CONFORM TO THE REQUIREMENT FOR CLASS AAA CONCRETE OF THE NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AS AVAILABLE AT: [HTTP://WWW.NH.GOV/DOT/ORG/PROJECTDEVELOPMENT/HIGHWAYDESIGN/SPECIFICATIONS/INDEX.HTM](http://www.nh.gov/dot/ORG/PROJECTDEVELOPMENT/HIGHWAYDESIGN/SPECIFICATIONS/INDEX.HTM)
2. SHELVES SHALL BE CONSTRUCTED TO THE ELEVATION OF THE HIGHEST PIPE CROWN AND SLOPED TO DRAIN TOWARD THE FLOWING THROUGH CHANNEL IN ACCORDANCE WITH ENV-WQ 704.12 (k).
3. ALL MANHOLES SHALL BE TESTED FOR LEAKAGE IN ACCORDANCE WITH ENV-WQ 704.17 (a) THROUGH (e).
4. SEWER MANHOLE COVERS SHALL CONFORM TO ASTM A48/48M WITH A CASTING EQUAL TO CLASS 30 IN ACCORDANCE WITH ENV-WQ 704.13 (b).
5. ALL PRECAST SECTIONS SHALL BE COATED ON THE EXTERIOR WITH A BITUMINOUS DAMP-PROOFING COATING IN ACCORDANCE WITH ENV-WQ 704.12 (j).
6. ALL PRECAST SECTIONS AND BASES SHALL HAVE THE DATE OF MANUFACTURE AND THE NAME OR TRADEMARK OF THE MANUFACTURER IMPRESSED OR INDUBLY MARKED ON THE INSIDE WALL PER ENV-WQ 704.12(i).
7. BRICK MASONRY SHALL CONFORM TO ASTM C32 (ENV-WQ 704.12(a)(9))

SEWER MANHOLE

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9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

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J/B Jones & Beach Engineers, Inc.

85 Portsmouth Ave. Stratham, NH 03885

Civil Engineering Services

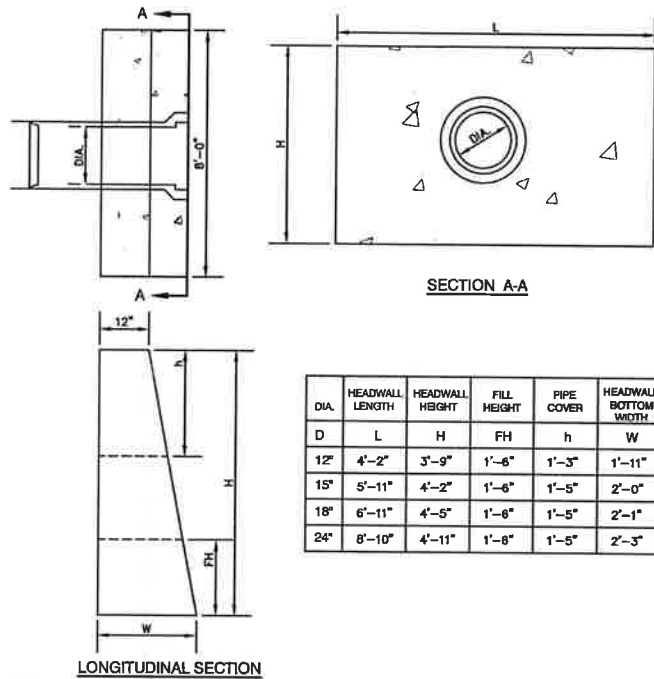
603-772-4746 FAX: 603-772-0227 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	DETAIL SHEET
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.

D2

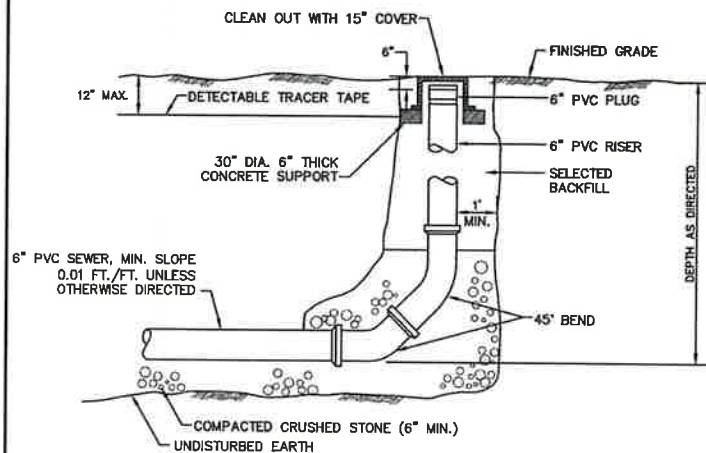
SHEET 41 OF 46
JBE PROJECT NO. 21090



- NOTES:**
1. ALL DIMENSIONS GIVEN IN FEET & INCHES.
 2. PROVIDE BELL END AT INLET HEADWALL, AND SPIGOT END AT OUTLET END HEADWALL.
 3. CONCRETE: 5,000 PSI MINIMUM AFTER 28 DAYS. CEMENT TO BE TYPE III PER ASTM C-150. REINFORCING TO MEET OR EXCEED ASTM A-615 GRADE 60 DEFORMED BARS.
 4. 1" THREADED INSERTS PROVIDED FOR FINAL ATTACHMENT IN FIELD BY OTHERS.

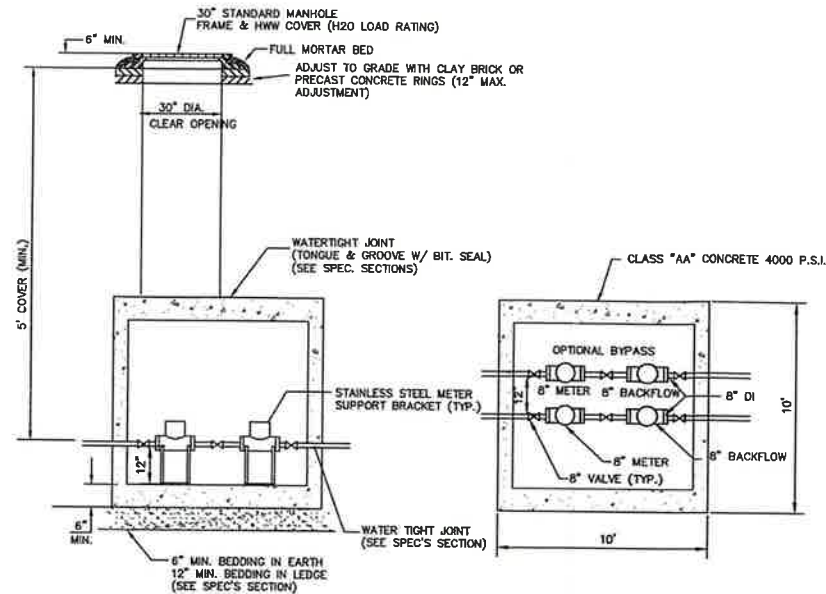
PRECAST CONCRETE HEADWALL

NOT TO SCALE



SEWER CLEAN OUT

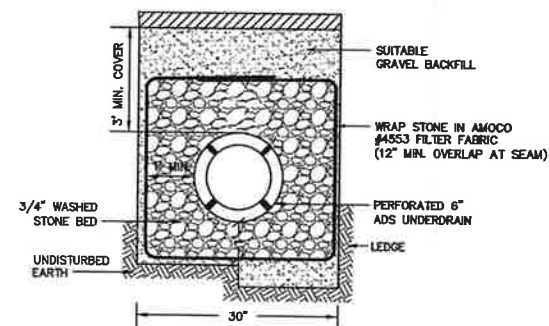
NOT TO SCALE



- NOTES:**
1. METER TO BE SENSUS OMNI C2 OF APPROPRIATE SIZE.
 2. BACKFLOW TO BE TESTABLE DOUBLE CHECK VALVE ASSEMBLY WITH CENTER-SHAFT OR TOP HINGE CHECKS (MILKINS 350AST OR EQUAL) OF APPROPRIATE SIZE, IF APPLICATION IS DESIGNED LOW HAZARD.
 3. OPTIONAL BYPASS MAY BE SIZED FOR DOMESTIC SERVICE ONLY.
 4. VAULT TO HAVE ADEQUATE ANTI-BOUYANCY FEATURES.
 5. VAULT COVERS TO BE STAMPED WITH "WATER" AND MATCH EXISTING CITY OF ROCHESTER INFRASTRUCTURE STANDARDS.
 6. ISOLATION VALVES REQUIRED AROUND EQUIPMENT FOR MAINTENANCE, TESTING AND SERVICE.
 7. IF APPLICATION IS DESIGNATED HIGH HAZARD, THE ASSEMBLY MUST USE RP2 BACKFLOW DEVICES AND BE LOCATED IN AN ABOVE GRADE, HEATED AND INSULATED ENCLOSURE TO ALLOW FOR DRAINING.
 8. VAULT AND ACCESS HATCHES SHALL BE SIZED TO ALLOW ENTRY FOR INSPECTION, TESTING AND COMPLETE REPLACEMENT OF DEVICES.
- BYPASS USAGE:**
9. IF OPTIONAL BYPASS LINE IS INSTALLED, WHEN MAIN FEED IS TAKEN OFFLINE AND BYPASS ENGAGED, OWNER SHALL CALL 330-7128 TO COORDINATE LOW FLOW CONDITION WITH ROCHESTER FIRE DEPARTMENT AND ROCHESTER PUBLIC WORKS DEPARTMENT.

WATER METER PIT ROCHESTER

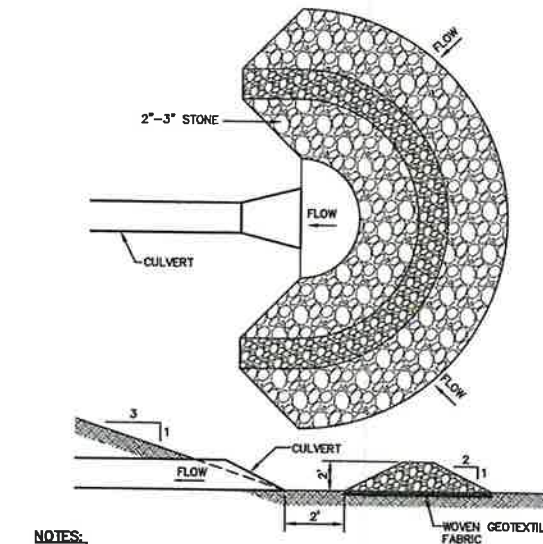
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- NOTES:**
1. PAVEMENT REPAIR IN EXISTING ROADWAYS SHALL CONFORM TO STREET OPENING REGULATIONS.
 2. NEW ROADWAY CONSTRUCTION SHALL CONFORM TO PROJECT AND CITY SPECIFICATIONS.
 3. SLOPE UNDERDRAIN PIPE TO DAYLIGHT.

ROADWAY UNDERDRAIN TRENCH

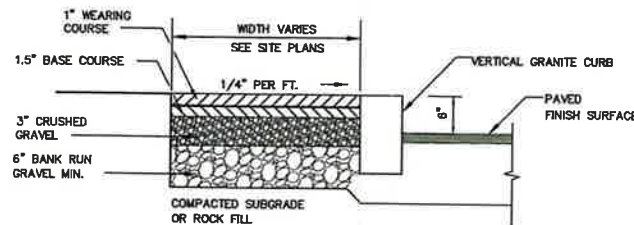
NOT TO SCALE



- NOTES:**
1. TEMPORARY CULVERT INLET PROTECTION CHECK DAMS SHALL BE CONSTRUCTED OF 2-3" STONE OVER WOVEN GEOTEXTILE FABRIC.
 2. INLET PROTECTION MEASURES SHALL BE INSTALLED AT THE OPENINGS OF ALL EXISTING AND PROPOSED CULVERTS LOCATED BELOW (DOWNSTREAM) FROM AND WITHIN 100' OF THE PROJECT SITE.
 3. SEDIMENT SHALL BE REMOVED FROM BEHIND THE STRUCTURE WHEN IT HAS ACCUMULATED TO ONE HALF THE ORIGINAL HEIGHT OF THE STRUCTURE.
 4. STRUCTURES SHALL BE REMOVED WHEN THE SITE IS STABILIZED WITH VEGETATION AND THE CHANNEL SHALL BE SMOOTHED AND REVEGETATED.

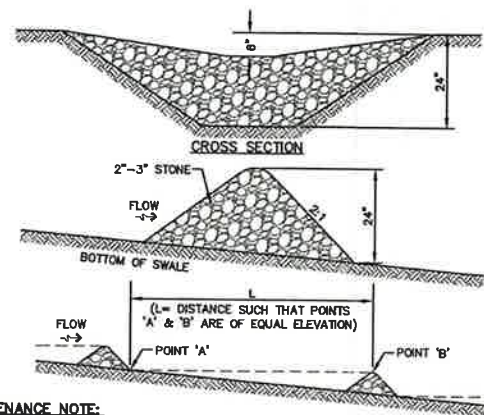
TEMPORARY CULVERT INLET PROTECTION CHECK DAM

NOT TO SCALE



BIT. SIDEWALK W/ VERTICAL GRANITE CURB

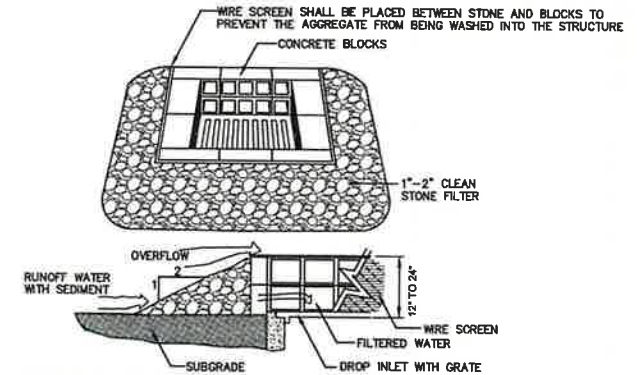
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- MAINTENANCE NOTE:**
1. STONE CHECK DAMS SHOULD BE CHECKED AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY NECESSARY REPAIRS SHOULD BE MADE IMMEDIATELY. PARTICULAR ATTENTION SHOULD BE GIVEN TO END RUN AND EROSION AT THE DOWNSTREAM TOE OF THE STRUCTURE. WHEN THE STRUCTURES ARE REMOVED, THE DISTURBED PORTION SHOULD BE BROUGHT TO THE EXISTING CHANNEL GRADE AND THE AREAS PREPARED, SEEDED AND MULCHED. WHILE THIS PRACTICE IS NOT INTENDED TO BE USED PRIMARILY FOR SEDIMENT TRAPPING, SOME SEDIMENT WILL ACCUMULATE BEHIND THE STRUCTURES. SEDIMENT SHALL BE REMOVED FROM BEHIND THE STRUCTURES WHEN IT HAS ACCUMULATED TO ONE HALF THE ORIGINAL HEIGHT OF THE STRUCTURE.

STONE CHECK DAM

NOT TO SCALE



- MAINTENANCE NOTE:**
1. ALL STRUCTURES SHOULD BE INSPECTED AFTER EVERY RAINFALL AND REPAIRS MADE AS NECESSARY. SEDIMENT SHOULD BE REMOVED FROM TRAPPING DEVICES AFTER THE SEDIMENT HAS REACHED A MAXIMUM OF ONE HALF THE DEPTH OF THE TRAP. THE SEDIMENT SHOULD BE DISPOSED IN A SUITABLE UPLAND AREA AND PROTECTED FROM EROSION BY EITHER STRUCTURE OR VEGETATIVE MEANS. THE TEMPORARY TRAPS SHOULD BE REMOVED AND THE AREA REPAIRED AS SOON AS THE CONTRIBUTING DRAINAGE AREA TO THE INLET HAS BEEN COMPLETELY STABILIZED.

TEMPORARY CATCH BASIN INLET PROTECTION (Block and Gravel Drop Inlet Sediment Filter)

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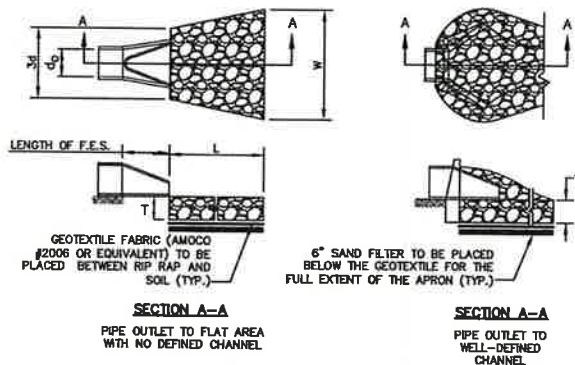


TABLE 7-24--RECOMMENDED RIP RAP GRADATION RANGES			
THICKNESS OF RIP RAP = 1.5 FEET			
d50 SIZE=	0.50	FEET	6 INCHES
% OF WEIGHT SMALLER THAN THE GIVEN d50 SIZE	SIZE OF STONE (INCHES) FROM TO		
100%	9	12	
85%	8	11	
50%	6	9	
15%	2	3	

- NOTES:**
1. THE SUBGRADE FOR THE GEOTEXTILE FABRIC AND RIP RAP SHALL BE PREPARED TO THE LINES AND GRADES SHOWN ON THE PLANS.
 2. THE RIP RAP SHALL CONFORM TO THE SPECIFIED GRADATION.
 3. GEOTEXTILE FABRICS SHALL BE PROTECTED FROM PUNCTURE OR TEARING DURING THE PLACEMENT OF THE RIP RAP. DAMAGED AREAS IN THE FABRIC SHALL BE REPAIRED BY PLACING A PIECE OF FABRIC OVER THE DAMAGED AREA OR BY COMPLETE REPLACEMENT OF THE FABRIC. ALL OVERLAPS REQUIRED FOR REPAIRS OR JOINING TWO PIECES OF FABRIC SHALL BE A MINIMUM OF 12 INCHES.
 4. STONE FOR THE RIP RAP MAY BE PLACED BY EQUIPMENT AND SHALL BE CONSTRUCTED TO THE FULL LAYER THICKNESS IN ONE OPERATION AND IN SUCH A MANNER AS TO PREVENT SEGREGATION OF THE STONE SIZES.
 5. OUTLETS TO A DEFINED CHANNEL SHALL HAVE 2:1 OR FLATTER SIDE SLOPES AND SHOULD BEGIN AT THE TOP OF THE CULVERT AND TAPER DOWN TO THE CHANNEL BOTTOM THROUGH THE LENGTH OF THE APRON.
 6. MAINTENANCE: THE OUTLET PROTECTION SHOULD BE CHECKED AT LEAST ANNUALLY AND AFTER EVERY MAJOR STORM. IF THE RIP RAP HAS BEEN DISPLACED, UNDERMINED OR DAMAGED, IT SHOULD BE REPAIRED IMMEDIATELY. THE CHANNEL IMMEDIATELY BELOW THE OUTLET SHOULD BE CHECKED TO SEE THAT EROSION IS NOT OCCURRING. THE DOWNSTREAM CHANNEL SHOULD BE KEPT CLEAR OF OBSTRUCTIONS SUCH AS FALLEN TREES, DEBRIS, AND SEDIMENT THAT COULD CHANGE FLOW PATTERNS AND/OR TAILWATER DEPTHS ON THE PIPES. REPAIRS MUST BE CARRIED OUT IMMEDIATELY TO AVOID ADDITIONAL DAMAGE TO OUTLET PROTECTION.

RIP RAP OUTLET PROTECTION APRON

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7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

Designed and Produced in NH

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Civil Engineering Services

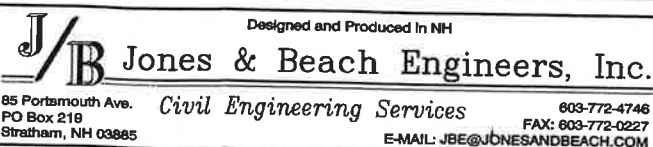
603-772-4746
FAX: 603-772-0227
E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	DETAIL SHEET
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Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

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D3

SHEET 42 OF 46
JBE PROJECT NO. 21090



BLASTING SPECIFICATIONS

A. BEST MANAGEMENT PRACTICES FOR BLASTING: ALL ACTIVITIES RELATED TO BLASTING SHALL FOLLOW BEST MANAGEMENT PRACTICES (BMPs) TO PREVENT CONTAMINATION OF GROUNDWATER INCLUDING PREPARING, REVIEWING AND FOLLOWING AN APPROVED BLASTING PLAN, PROPER DRILLING, EXPLOSIVE HANDLING AND LOADING PROCEDURES; OBSERVING THE ENTIRE BLASTING PROCEDURE; EVALUATING BLASTING PERFORMANCE; AND HANDLING AND STORAGE OF BLASTED ROCK.

- (1) **LOADING PRACTICES:** THE FOLLOWING BLASTHOLE LOADING PRACTICES TO MINIMIZE ENVIRONMENTAL EFFECTS SHALL BE FOLLOWED:
 - (a) DRILLING LOGS SHALL BE MAINTAINED BY THE DRILLER AND COMMUNICATED DIRECTLY TO THE BLASTER. THE LOGS SHALL INDICATE DEPTHS AND LENGTHS OF VOIDS, CAVITIES, AND FAULT ZONES OR OTHER WEAK ZONES ENCOUNTERED AS WELL AS GROUNDWATER CONDITIONS.
 - (b) EXPLOSIVE PRODUCTS SHALL BE MANAGED ON-SITE SO THAT THEY ARE EITHER USED IN THE BOREHOLE, RETURNED TO THE DELIVERY VEHICLE, OR PLACED IN SECURE CONTAINERS FOR OFF-SITE DISPOSAL.
 - (c) SPILLAGE AROUND THE BOREHOLE SHALL EITHER BE PLACED IN THE BOREHOLE OR CLEANED UP AND RETURNED TO AN APPROPRIATE VEHICLE FOR HANDLING OR PLACEMENT IN SECURED CONTAINERS FOR OFF-SITE DISPOSAL.
 - (d) LOADED EXPLOSIVES SHALL BE DETONATED AS SOON AS POSSIBLE AND SHALL NOT BE LEFT IN THE BLASTHOLES OVERNIGHT, UNLESS WEATHER OR OTHER SAFETY CONCERNS REASONABLY DICTATE THAT DETONATION SHOULD BE POSTPONED.
 - (e) LOADING EQUIPMENT SHALL BE CLEANED IN AN AREA WHERE WASTEWATER CAN BE PROPERLY CONTAINED AND HANDLED IN A MANNER THAT PREVENTS RELEASE OF CONTAMINANTS TO THE ENVIRONMENT.
 - (f) EXPLOSIVES SHALL BE LOADED TO MAINTAIN GOOD CONTINUITY IN THE COLUMN LOAD TO PROMOTE COMPLETE DETONATION. INDUSTRY ACCEPTED LOADING PRACTICES FOR PRIMING, STEMMING, DECKING AND COLUMN RISE NEED TO BE ATTENDED TO.

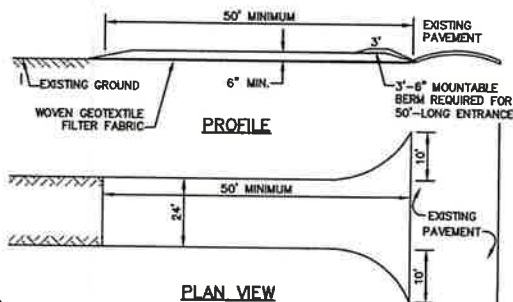
- (2) **EXPLOSIVE SELECTION:** THE FOLLOWING BMPs SHALL BE FOLLOWED TO REDUCE THE POTENTIAL FOR GROUNDWATER CONTAMINATION WHEN EXPLOSIVES ARE USED:
 - (a) EXPLOSIVE PRODUCTS SHALL BE SELECTED THAT ARE APPROPRIATE FOR SITE CONDITIONS AND SAFE BLAST DETECTION.
 - (b) EXPLOSIVE PRODUCTS SHALL BE SELECTED THAT HAVE THE APPROPRIATE WATER RESISTANCE FOR THE SITE CONDITIONS PRESENT TO MINIMIZE THE POTENTIAL FOR HAZARDOUS EFFECT OF THE PRODUCT UPON GROUNDWATER.

- (3) **PREVENTION OF MISFIRE:** APPROPRIATE PRACTICES SHALL BE DEVELOPED AND IMPLEMENTED TO PREVENT MISFIRES.

- (4) **MUCK PILE MANAGEMENT:** MUCK PILES (THE BLASTED PIECES OF ROCK) AND ROCK PILES SHALL BE MANAGED IN A MANNER TO REDUCE THE POTENTIAL FOR CONTAMINATION BY IMPLEMENTING THE FOLLOWING MEASURES:
 - (a) REMOVE THE MUCK PILE FROM THE BLAST AREA AS SOON AS REASONABLY POSSIBLE.
 - (b) MANAGE THE INTERACTION OF BLASTED ROCK PILES AND STORMWATER TO PREVENT CONTAMINATION OF WATER SUPPLY WELLS OR SURFACE WATER.

- (5) **SPILL PREVENTION MEASURES AND SPILL MITIGATION:** SPILL PREVENTION AND SPILL MITIGATION MEASURES SHALL BE IMPLEMENTED TO PREVENT THE RELEASE OF FUEL AND OTHER RELATED SUBSTANCES TO THE ENVIRONMENT. THE MEASURES SHALL INCLUDE AT A MINIMUM:
 - a. THE FUEL STORAGE REQUIREMENTS SHALL INCLUDE:
 1. STORAGE OF REGULATED SUBSTANCES ON AN IMPERVIOUS SURFACE.
 2. SECURE STORAGE AREAS AGAINST UNAUTHORIZED ENTRY.
 3. LABEL REGULATED CONTAINERS CLEARLY AND VISIBLY.
 4. INSPECT STORAGE AREAS WEEKLY.
 5. COVER REGULATED CONTAINERS IN OUTSIDE STORAGE AREAS.
 6. WHEREVER POSSIBLE, KEEP REGULATED CONTAINERS THAT ARE STORED OUTSIDE MORE THAN 50 FEET FROM SURFACE WATER AND STORM DRAINS, 75 FEET FROM PRIVATE WELLS, AND 400 FEET FROM PUBLIC WELLS.
 7. SECONDARY CONTAINMENT IS REQUIRED FOR CONTAINERS CONTAINING REGULATED SUBSTANCES STORED OUTSIDE, EXCEPT FOR ON PREMISE USE HEATING FUEL TANKS, OR ABOVEGROUND OR UNDERGROUND STORAGE TANKS OTHERWISE REGULATED.

- b. THE FUEL HANDLING REQUIREMENTS SHALL INCLUDE:
 1. EXCEPT WHEN IN USE, KEEP CONTAINERS CONTAINING REGULATED SUBSTANCES CLOSED AND SEALED.
 2. PLACE DRIP PANS UNDER SPIGOTS, VALVES, AND PUMPS.
 3. HAVE SPILL CONTROL AND CONTAINMENT EQUIPMENT READILY AVAILABLE IN ALL WORK AREAS.
 4. USE FUNNELS AND DRIP PANS WHEN TRANSFERRING REGULATED SUBSTANCES.
 5. PERFORM TRANSFERS OF REGULATED SUBSTANCES OVER AN IMPERVIOUS SURFACE.
- c. THE TRAINING OF ON-SITE EMPLOYEES AND THE ON-SITE POSTING OF RELEASE RESPONSE INFORMATION DESCRIBING WHAT TO DO IN THE EVENT OF A SPILL OF REGULATED SUBSTANCES.
- d. FUELING AND MAINTENANCE OF EXCAVATION, EARTHMOVING AND OTHER CONSTRUCTION RELATED EQUIPMENT WILL COMPLY WITH THE REGULATIONS OF NHDES [NOTE THESE REQUIREMENTS ARE SUMMARIZED IN WD 008-22-6: "BEST MANAGEMENT PRACTICES FOR FUELING AND MAINTENANCE OF EXCAVATION AND EARTHMOVING EQUIPMENT" OR ITS SUCCESSOR DOCUMENT].



NOTES:

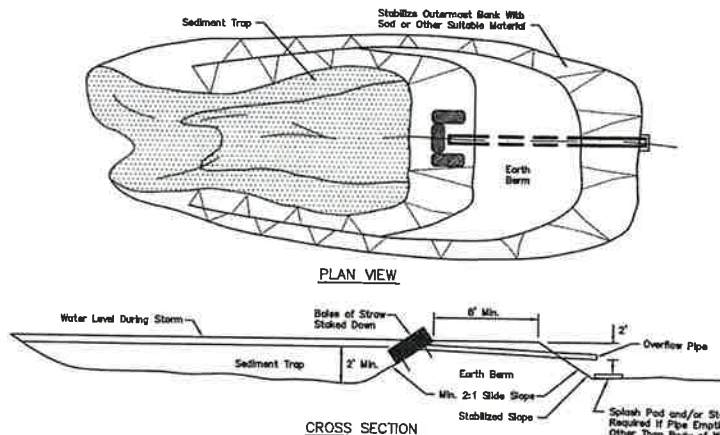
1. STONE FOR STABILIZED CONSTRUCTION ENTRANCE SHALL BE 3 INCH STONE, RECLAIMED STONE, OR RECYCLED CONCRETE EQUIVALENT.
2. THE LENGTH OF THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 50 FEET, EXCEPT FOR A SINGLE RESIDENTIAL LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY.
3. THICKNESS OF THE STONE FOR THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 6 INCHES.
4. THE WIDTH OF THE ENTRANCE SHALL NOT BE LESS THAN THE FULL WIDTH OF THE ENTRANCE WHERE INGRESS OR EGRESS OCCURS, OR 10 FEET, WHICHEVER IS GREATER.
5. GEOTEXTILE FILTER FABRIC SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING THE STONE.
6. FILTER FABRIC IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENTIAL LOT.
7. ALL SURFACE WATER THAT IS FLOWING TO OR DIVERTED TOWARD THE CONSTRUCTION ENTRANCE SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A STONE BERM WITH 5:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PIPE.
8. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO THE PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED, OR TRACKED ONTO THE PUBLIC RIGHT-OF-WAY MUST BE REMOVED PROMPTLY.

STABILIZED CONSTRUCTION ENTRANCE

NOT TO SCALE

TEMPORARY EROSION CONTROL NOTES

1. THE SMALLEST PRACTICAL AREA OF LAND SHALL BE EXPOSED AT ANY ONE TIME. AT NO TIME SHALL AN AREA IN EXCESS OF 5 ACRES BE EXPOSED AT ANY ONE TIME BEFORE DISTURBED AREAS ARE STABILIZED.
2. EROSION, SEDIMENT AND DETENTION MEASURES SHALL BE INSTALLED AS SHOWN ON THE PLANS AND AT LOCATIONS AS REQUIRED, DIRECTED BY THE ENGINEER.
3. ALL DISTURBED AREAS (INCLUDING POND AREAS BELOW THE PROPOSED WATERLINE) SHALL BE RETURNED TO PROPOSED GRADES AND ELEVATIONS. DISTURBED AREAS SHALL BE LOAMED WITH A MINIMUM OF 6" OF SCREENED ORGANIC LOAM AND SEEDING WITH SEED MIXTURE "C" AT A RATE NOT LESS THAN 1.10 POUNDS OF SEED PER 1,000 S.F. OF AREA (48 LBS. / ACRE).
4. SILT FENCES AND OTHER BARRIERS SHALL BE INSPECTED EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 0.5" OR GREATER. ALL DAMAGED AREAS SHALL BE REPAIRED, AND SEDIMENT DEPOSITS SHALL PERIODICALLY BE REMOVED AND DISPOSED OF.
5. AFTER ALL DISTURBED AREAS HAVE BEEN STABILIZED, THE TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED AND THE AREA DISTURBED BY THE REMOVAL SMOOTHED AND RE-VEGETATED.
6. AREAS MUST BE SEED AND MULCHED OR OTHERWISE PERMANENTLY STABILIZED WITHIN 3 DAYS OF FINAL GRADING, OR TEMPORARILY STABILIZED WITHIN 14 DAYS OF THE INITIAL DISTURBANCE OF SOIL. ALL AREAS SHALL BE STABILIZED WITHIN 45 DAYS OF INITIAL DISTURBANCE.
7. IN AREAS WHERE FINAL GRADING HAS NOT OCCURRED, TEMPORARY STABILIZATION MEASURES SHOULD BE IN PLACE WITHIN 5 CALENDAR DAYS FOR EXPOSED SOIL AREAS THAT ARE WITHIN 100 FEET OF A SURFACE WATERBODY OR A WETLAND AND NO MORE THAN 14 CALENDAR DAYS FOR ALL OTHER AREAS. PERMANENT STABILIZATION SHOULD BE IN PLACE WITHIN 3 CALENDAR DAYS FOLLOWING COMPLETION OF FINAL GRADING OF EXPOSED SOIL AREAS.
8. ALL PROPOSED VEGETATED AREAS THAT DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED BY SEEDING AND INSTALLING NORTH AMERICAN GREEN S75 EROSION CONTROL BLANKETS (OR AN EQUIVALENT APPROVED IN WRITING BY THE ENGINEER) ON SLOPES GREATER THAN 3:1, AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE SECURED WITH ANCHORED NETTING. ELSEWHERE, THE INSTALLATION OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR ON FROZEN GROUND AND SHALL BE COMPLETED IN ADVANCE OF THAW OR SPRING MELT EVENTS.
9. ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85 PERCENT VEGETATIVE GROWTH BY OCTOBER 15, OR WHICH ARE DISTURBED AFTER OCTOBER 15, SHALL BE STABILIZED TEMPORARILY WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE DESIGN FLOW CONDITIONS.
10. AFTER OCTOBER 15th, INCOMPLETE ROAD OR PARKING SURFACES, WHERE WORK HAS STOPPED FOR THE WINTER SEASON, SHALL BE PROTECTED WITH A MINIMUM OF 3" OF CRUSHED GRAVEL PER NHDOT ITEM 304.3.
11. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:
 - a. BASE COURSE GRAVELS HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
 - b. A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
 - c. A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH STONE OR RIPRAP HAS BEEN INSTALLED; OR
 - d. EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.
12. FUGITIVE DUST CONTROL IS REQUIRED TO BE CONTROLLED IN ACCORDANCE WITH ENV-A 1000, AND THE PROJECT IS TO MEET THE REQUIREMENTS AND INTENT OF RSA 430:53 AND AGR 3800 RELATIVE TO INVASIVE SPECIES.
13. PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR'S NAME, ADDRESS, AND PHONE NUMBER SHALL BE SUBMITTED TO DES VIA EMAIL (SEE BELOW).
14. PRIOR TO CONSTRUCTION, A PHASING PLAN THAT DELINEATES EACH PHASE OF THE PROJECT SHALL BE SUBMITTED. ALL TEMPORARY SEDIMENT BASINS THAT WILL BE NEEDED FOR DEWATERING WORK AREAS SHALL BE LOCATED AND IDENTIFIED ON THIS PLAN.
15. IN ORDER TO ENSURE THE STABILITY OF THE SITE AND EFFECTIVE IMPLEMENTATION OF THE SEDIMENT AND EROSION CONTROL MEASURES SPECIFIED IN THE PLANS FOR THE DURATION OF CONSTRUCTION, THE CONTRACTOR SHALL BE IN STRICT COMPLIANCE WITH THE FOLLOWING INSPECTION AND MAINTENANCE REQUIREMENTS IN ADDITION TO THOSE CALLED FOR IN THE SNMPP:
 - a. A CERTIFIED PROFESSIONAL IN EROSION AND SEDIMENT CONTROL OR A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF NEW HAMPSHIRE ("MONITOR") SHALL BE EMPLOYED TO INSPECT THE SITE FROM THE START OF ALTERATION OF TERRAIN ACTIVITIES UNTIL THE SITE IS IN FULL COMPLIANCE WITH THE SITE SPECIFIC PERMIT ("PERMIT").
 - b. DURING THIS PERIOD, THE MONITOR SHALL INSPECT THE SUBJECT SITE AT LEAST ONCE A WEEK, AND IF POSSIBLE, DURING ANY 1/2 INCH OR GREATER RAIN EVENT (I.E. 1/2 INCH OF PRECIPITATION OR MORE WITHIN A 24 HOUR PERIOD). IF UNABLE TO BE PRESENT DURING SUCH A STORM, THE MONITOR SHALL INSPECT THE SITE WITHIN 24 HOURS OF THIS EVENT.
 - c. THE MONITOR SHALL PROVIDE TECHNICAL ASSISTANCE AND RECOMMENDATIONS TO THE CONTRACTOR ON THE APPROPRIATE BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROLS REQUIRED TO MEET THE REQUIREMENTS OF RSA 485 A:17 AND ALL APPLICABLE DES PERMIT CONDITIONS.
 - d. WITHIN 24 HOURS OF EACH INSPECTION, THE MONITOR SHALL SUBMIT A REPORT TO DES VIA EMAIL (RIDGELY MAUCK AT: RIDGELYMAUCK@DES.NH.GOV).
 - e. THE MONITOR SHALL MEET WITH DES TO DECIDE UPON A REPORT FORMAT. THE REPORT FORMAT SHALL BE REVIEWED AND APPROVED BY DES PRIOR TO THE START OF CONSTRUCTION.
 - f. THE MONITOR SHALL INCLUDE PHOTOGRAPHS OF THE SITE THAT ARE REPRESENTATIVE OF THE PROJECT.



TEMPORARY SEDIMENT BASIN

NOT TO SCALE

SEEDING SPECIFICATIONS

1. **GRADING AND SHAPING**
 - A. SLOPES SHALL NOT BE STEEPER THAN 2:1 WITHOUT APPROPRIATE EROSION CONTROL MEASURES AS SPECIFIED ON THE PLANS (3:1 SLOPES OR FLATTER ARE PREFERRED).
 - B. WHERE MOWING WILL BE DONE, 3:1 SLOPES OR FLATTER ARE RECOMMENDED.
2. **SEEDBED PREPARATION**
 - A. SURFACE AND SEEPAGE WATER SHOULD BE DRAINED OR DIVERTED FROM THE SITE TO PREVENT DROWNING OR WINTER KILLING OF THE PLANTS.
 - B. STONES LARGER THAN 4 INCHES AND TRASH SHOULD BE REMOVED BECAUSE THEY INTERFERE WITH SEEDING AND FUTURE MAINTENANCE OF THE AREA. WHERE FEASIBLE, THE SOIL SHOULD BE TILLED TO A DEPTH OF ABOUT 4 INCHES TO PREPARE A SEEDBED AND FERTILIZER AND LIME MIXED INTO THE SOIL. THE SEEDBED SHOULD BE LEFT IN A REASONABLY FIRM AND SMOOTH CONDITION. THE LAST TILLAGE OPERATION SHOULD BE PERFORMED ACROSS THE SLOPE WHEREVER PRACTICAL.
3. **ESTABLISHING A STAND**
 - A. LIME AND FERTILIZER SHOULD BE APPLIED PRIOR TO OR AT THE TIME OF SEEDING AND INCORPORATED INTO THE SOIL. TYPES AND AMOUNTS OF LIME AND FERTILIZER SHOULD BE BASED ON AN EVALUATION OF SOIL TESTS. WHEN A SOIL TEST IS NOT AVAILABLE, THE FOLLOWING MINIMUM AMOUNTS SHOULD BE APPLIED:

AGRICULTURAL LIMESTONE	2 TONS PER ACRE OR 100 LBS. PER 1,000 SQ.FT.
NITROGEN(N),	50 LBS. PER ACRE OR 1.1 LBS. PER 1,000 SQ.FT.
PHOSPHATE(P2O5),	100 LBS. PER ACRE OR 2.2 LBS. PER 1,000 SQ.FT.
POTASH(K2O),	100 LBS. PER ACRE OR 2.2 LBS. PER 1,000 SQ.FT.

 (NOTE: THIS IS THE EQUIVALENT OF 500 LBS. PER ACRE OF 10-20-20 FERTILIZER OR 1,000 LBS. PER ACRE OF 5-10-10.)
 - B. SEED SHOULD BE SPREAD UNIFORMLY BY THE METHOD MOST APPROPRIATE FOR THE SITE. METHODS INCLUDE BROADCASTING, DRILLING AND HYDROSEEDING. WHERE BROADCASTING IS USED, COVER SEED WITH .25 INCH OF SOIL OR LESS, BY CULTIPACKING OR RAKING.
 - C. REFER TO THE "SEEDING GUIDE" AND "SEEDING RATES" TABLES ON THIS SHEET FOR APPROPRIATE SEED MIXTURES AND RATES OF SEEDING. ALL LEGUMES (CROWNFETCH, BROSFLO, AND FLATPEA) MUST BE INOCULATED WITH THEIR SPECIFIC INOCULANT PRIOR TO THEIR INTRODUCTION TO THE SITE.
 - D. WHEN SEEDING AREAS ARE MULCHED, PLANTINGS MAY BE MADE FROM EARLY SPRING TO EARLY OCTOBER. WHEN SEEDING AREAS ARE NOT MULCHED, PLANTINGS SHOULD BE MADE FROM EARLY SPRING TO MAY 20th OR FROM AUGUST 10th TO SEPTEMBER 1st.
4. **MULCH**
 - A. HAY, STRAW, OR OTHER MULCH, WHEN NEEDED, SHOULD BE APPLIED IMMEDIATELY AFTER SEEDING.
 - B. MULCH WILL BE HELD IN PLACE USING APPROPRIATE TECHNIQUES FROM THE BEST MANAGEMENT PRACTICE FOR MULCHING. HAY OR STRAW MULCH SHALL BE PLACED AT A RATE OF 80 LBS PER 1000 S.F.
5. **MAINTENANCE TO ESTABLISH A STAND**
 - A. PLANTED AREAS SHOULD BE PROTECTED FROM DAMAGE BY FIRE, GRAZING, TRAFFIC, AND DENSE WEED GROWTH.
 - B. FERTILIZATION NEEDS SHOULD BE DETERMINED BY ON-SITE INSPECTIONS. SUPPLEMENTAL FERTILIZER IS USUALLY THE KEY TO FULLY COMPLETE THE ESTABLISHMENT OF THE STAND BECAUSE MOST PERENNIALS TAKE 2 TO 3 YEARS TO BECOME FULLY ESTABLISHED.
 - C. IN WATERWAYS, CHANNELS, OR SWALES WHERE UNIFORM FLOW CONDITIONS ARE ANTICIPATED, ANNUAL MOWING MAY BE NECESSARY TO CONTROL GROWTH OF WOODY VEGETATION.

USE	SEEDING MIXTURE 1/	DROUGHTY	WELL DRAINED	MODERATELY WELL DRAINED	POORLY DRAINED
STEEP CUTS AND FILLS, BORROW AND DISPOSAL AREAS	A B C D	FAIR POOR POOR FAIR	GOOD GOOD GOOD EXCELLENT	GOOD FAIR EXCELLENT EXCELLENT	FAIR FAIR GOOD POOR
WATERWAYS, EMERGENCY SPILLWAYS, AND OTHER CHANNELS WITH FLOWING WATER	A C	GOOD GOOD	GOOD EXCELLENT	GOOD EXCELLENT	FAIR FAIR
LIGHTLY USED PARKING LOTS, ODD AREAS, UNUSED LANDS, AND LOW INTENSITY USE RECREATION SITES	A B C	GOOD GOOD GOOD	GOOD GOOD EXCELLENT	GOOD FAIR EXCELLENT	FAIR POOR FAIR
PLAY AREAS AND ATHLETIC FIELDS (TOPSOIL IS ESSENTIAL FOR GOOD TURF.)	E F	FAIR FAIR	EXCELLENT EXCELLENT	EXCELLENT EXCELLENT	2/2 2/2

GRAVEL PIT, SEE NH-PW-24 IN APPENDIX FOR RECOMMENDATION REGARDING RECLAMATION OF SAND AND GRAVEL PITS.
1/ REFER TO SEEDING MIXTURES AND RATES IN TABLE BELOW.
2/ POORLY DRAINED SOILS ARE NOT DESIRABLE FOR USE AS PLAYING AREA AND ATHLETIC FIELDS.

NOTE: TEMPORARY SEED MIX FOR STABILIZATION OF TURF SHALL BE WINTER RYE OR OATS AT A RATE OF 2.5 LBS. PER 1000 S.F. AND SHALL BE PLACED PRIOR TO OCTOBER 15th, IF PERMANENT SEEDING NOT YET COMPLETE.

SEEDING GUIDE

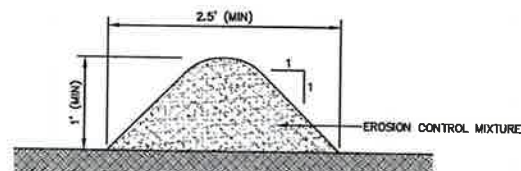
MIXTURE	POUNDS PER ACRE	POUNDS PER 1,000 SQ. FT.
A. TALL FESCUE	20	0.45
CREeping RED FESCUE	20	0.45
RED TOP	2	0.05
TOTAL	42	0.95
B. TALL FESCUE	15	0.35
CREeping RED FESCUE	10	0.25
CROWN VETCH	15	0.35
OR FLAT PEA	30	0.75
TOTAL	40 OR 55	0.95 OR 1.35
C. TALL FESCUE	20	0.45
CREeping RED FESCUE	20	0.45
BIRDS FOOT TREFOIL	2	0.05
TOTAL	42	1.10
D. TALL FESCUE	20	0.45
FLAT PEA	30	0.75
TOTAL	50	1.20
E. CREeping RED FESCUE 1/	50	1.15
KENTUCKY BLUEGRASS 1/2	50	1.15
TOTAL	100	2.30
F. TALL FESCUE 1	150	3.60

1/ FOR HEAVY USE ATHLETIC FIELDS CONSULT THE UNIVERSITY OF NEW HAMPSHIRE COOPERATIVE EXTENSION TURF SPECIALIST FOR CURRENT VARIETIES AND SEEDING RATES.

SEEDING RATES

CONSTRUCTION SEQUENCE

1. PRIOR TO THE START OF ANY ACTIVITY, IT IS THE RESPONSIBILITY OF THE SITE'S SITE DEVELOPER (OR OWNER) TO FILE A NOTICE OF INTENT (NOI) FORM WITH THE ENVIRONMENTAL PROTECTION AGENCY (EPA) IN ORDER TO OBTAIN COVERAGE UNDER THE NPDES GENERAL PERMIT FOR STORM WATER DISCHARGES FROM CONSTRUCTION ACTIVITIES. A PRE CONSTRUCTION MEETING IS TO BE HELD WITH ALL DEPARTMENT HEADS PRIOR TO THE START OF CONSTRUCTION.
2. THIS PROJECT SHALL BE CONSTRUCTED ACCORDING TO THE PHASING PLAN INCLUDED WITHIN THE PLAN SET.
3. WETLAND BOUNDARIES ARE TO BE CLEARLY MARKED PRIOR TO THE START OF CONSTRUCTION.
4. CUT AND REMOVE TREES IN CONSTRUCTION AREA AS REQUIRED OR DIRECTED.
5. INSTALL SILT FENCING, HAY BALES AND CONSTRUCTION ENTRANCES PRIOR TO THE START OF CONSTRUCTION. THESE ARE TO BE MAINTAINED UNTIL THE FINAL PAVEMENT SURFACING AND LANDSCAPING AREAS ARE ESTABLISHED.
6. CLEAR, CUT, GRUB AND DISPOSE OF DEBRIS IN APPROVED FACILITIES. THIS INCLUDES ANY REQUIRED DEMOLITION OF EXISTING STRUCTURES, UTILITIES, ETC.
7. CONSTRUCT AND/OR INSTALL TEMPORARY OR PERMANENT SEDIMENT AND/OR DETENTION BASIN(S) AS REQUIRED PRIOR TO ROUGH GRADING. THESE FACILITIES SHALL BE INSTALLED AND STABILIZED PRIOR TO DIRECTING RUN-OFF TO THEM.
8. STRIP LOAM AND PAVEMENT, OR RECLAIM EXISTING PAVEMENT WITHIN LIMITS OF WORK PER THE RECOMMENDATIONS OF THE PROJECT ENGINEER AND STOCKPILE EXCESS MATERIAL. STABILIZE STOCKPILE AS NECESSARY.
9. PERFORM PRELIMINARY SITE GRADING IN ACCORDANCE WITH THE PLANS, INCLUDING THE CONSTRUCTION OF ANY RETAINING WALLS.
10. PREPARE BUILDING PAD(S) TO ENABLE BUILDING CONSTRUCTION TO BEGIN.
11. INSTALL THE SEWER AND DRAINAGE SYSTEMS FIRST, THEN ANY OTHER UTILITIES IN ACCORDANCE WITH THE PLAN AND DETAILS. ANY CONFLICTS BETWEEN UTILITIES ARE TO BE RESOLVED WITH THE INVOLVEMENT AND APPROVAL OF THE ENGINEER.
12. INSTALL INLET PROTECTION AT ALL CATCH BASINS AS THEY ARE CONSTRUCTED IN ACCORDANCE WITH DETAILS.
13. ALL SWALES AND DRAINAGE STRUCTURES ARE TO BE CONSTRUCTED AND STABILIZED PRIOR TO HAVING RUN-OFF DIRECTED TO THEM.
14. DAILY, OR AS REQUIRED, CONSTRUCT TEMPORARY BERMS, DRAINAGE DITCHES, CHECK DAMS, SEDIMENT TRAPS, ETC., TO PREVENT EROSION ON THE SITE AND PREVENT ANY SILTATION OF ADJUTING WATERS AND/OR PROPERTY.
15. PERFORM FINAL FINE GRADING, INCLUDING PLACEMENT OF "SELECT" SUBGRADE MATERIALS.
16. PAVE ALL PARKING LOTS AND ROADWAYS WITH INITIAL "BASE COURSE".
17. PERFORM ALL REMAINING SITE CONSTRUCTION (I.E. BUILDING, CURBING, UTILITY CONNECTIONS, ETC.).
18. LOAM AND SEED ALL DISTURBED AREAS AND INSTALL ANY REQUIRED SEDIMENT AND EROSION CONTROL FACILITIES (I.E. RIP RAP, EROSION CONTROL BLANKETS, ETC.).
19. FINISH PAVING ALL ROADWAYS AND PARKING AREAS WITH "FINISH" COURSE.
20. ALL ROADWAYS AND PARKING LOTS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
21. ALL CUT AND FILL SLOPES SHALL BE SEEDING/LOAMED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
22. COMPLETE PERMANENT SEEDING AND LANDSCAPING.
23. REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER SEEDING AREAS HAVE BEEN 75%-85% ESTABLISHED AND SITE IMPROVEMENTS ARE COMPLETE. SMOOTH AND RE-VEGETATE ALL DISTURBED AREAS.
24. CLEAN SITE AND ALL DRAINAGE STRUCTURES, PIPES AND SLUMPS OF ALL SILT AND DEBRIS.
25. INSTALL ALL PAINTED PAVEMENT MARKINGS AND SIGNAGE PER THE PLANS AND DETAILS.
26. ALL EROSION CONTROLS SHALL BE INSPECTED WEEKLY AND AFTER EVERY HALF-INCH OF RAINFALL.
27. UPON COMPLETION OF CONSTRUCTION, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY ANY RELEVANT PERMITTING AGENCIES THAT THE CONSTRUCTION HAS BEEN FINISHED IN A SATISFACTORY MANNER.



NOTES:

1. EROSION CONTROL MIX BERMS SHALL BE:
 - a. USED ONLY IN AREAS WHERE EROSION WILL OCCUR IN THE FORM OF SHEET EROSION ONLY AND THERE IS NO CONCENTRATION OF WATER IN A CHANNEL OR OTHER DRAINAGE WAY ABOVE THE BERM;
 - b. INSTALLED FOLLOWING THE CONTOUR OF THE LAND AS CLOSELY AS POSSIBLE;
 - c. USED ONLY IF THE AREA UPOPE OF THE BERM HAS A SLOPE OF LESS THAN 5% AND
 - d. AT LEAST 12 INCHES HIGH AND AT LEAST 2 FEET WIDE.
2. THE EROSION CONTROL MIX USED IN THE FILTER BERMS SHALL BE A WELL-GRADED MIXTURE OF PARTICLE SIZES, AND SHALL MEET THE FOLLOWING STANDARDS:
 - a) THE ORGANIC CONTENT SHALL BE 25-65% OF DRY WEIGHT THAT IS
 - 1) FIBROUS AND ELONGATED SUCH AS FROTH SHREDDED BARK, STUMP GRINDINGS, COMPOSTED BARK OR EQUIVALENT MANUFACTURED PRODUCTS; AND
 - 2) NOT COMPRISED OF WOOD CHIPS, BARK CHIPS, GROUND CONSTRUCTION DEBRIS OR REPROCESSED WOOD PRODUCTS
 - b) PARTICLE SIZE BY WEIGHT SHALL BE 100% PASSING A 3" SCREEN, 90% TO 100% PASSING A 1" SCREEN, 70% TO 100% PASSING A 0.75" SCREEN, AND 30%-75% PASSING A 0.25" SCREEN
 - c) DOES NOT CONTAIN SILTS, CLAYS OR FINE SANDS
 - d) THE PH SHALL BE BETWEEN 5.0 AND 8.0.
3. ORGANIC FILTER BERMS SHALL BE INSTALLED ALONG A RELATIVELY LEVEL CONTOUR, FOLLOWING THE CONTOUR OF THE LAND AS CLOSELY AS POSSIBLE. IT MAY BE NECESSARY TO CUT TALL GRASSES OR WOODY VEGETATION TO AVOID CREATING VOIDS AND BRIDGES THAT WOULD ENABLE FINES TO WASH UNDER THE BERM.
5. FROZEN GROUND, OUTCROPS OF BEDROCK, AND VERY ROOTED FORESTED AREAS PRESENT THE MOST PRACTICAL AND EFFECTIVE LOCATIONS FOR ORGANIC FILTER BERMS. OTHER BMP'S SHOULD BE USED AT LOW POINTS OF CONCENTRATED RUNOFF, BELOW CULVERT OUTLET APRONS, AROUND CATCH BASINS, AND AT THE BOTTOM OF STEEP PERIMETER SLOPES THAT HAVE A LARGE CONTRIBUTING AREA.
6. SEDIMENT SHALL BE REMOVED FROM BEHIND THE STRUCTURES WHEN IT HAS ACCUMULATED TO ONE HALF THE ORIGINAL HEIGHT OF THE STRUCTURE.
7. STRUCTURES MAY BE LEFT IN PLACE ONCE THE SITE IS STABILIZED.

ORGANIC FILTER BERM

NOT TO SCALE

Design: JAC	Draft: LAZ	Date: 04/29/21
Checked: JAC	Scale: AS NOTED	Project No.: 21090
Drawing Name: 21090-PLAN.dwg		

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REV.	DATE	REVISION	BY
10	9/8/22	REVISED PER CITY & NHDES Aot COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES Aot COMMENTS	LAZ
8	6/3/22	REVISED PER NHDES SEWER COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	6/29/22	REVISED PER TRAFFIC COMMENTS	LAZ

Designed and Produced in NH

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Civil Engineering Services

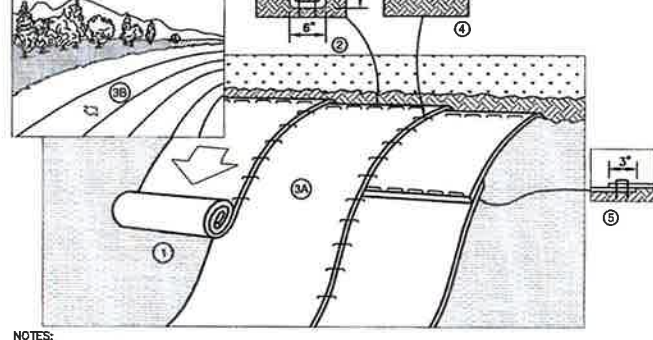
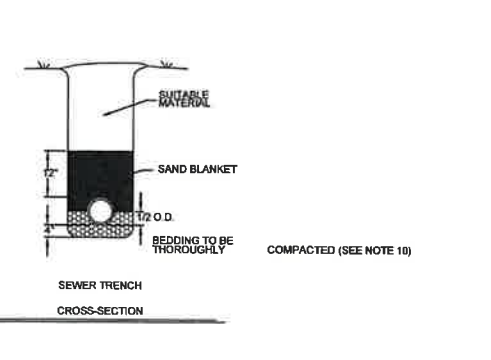
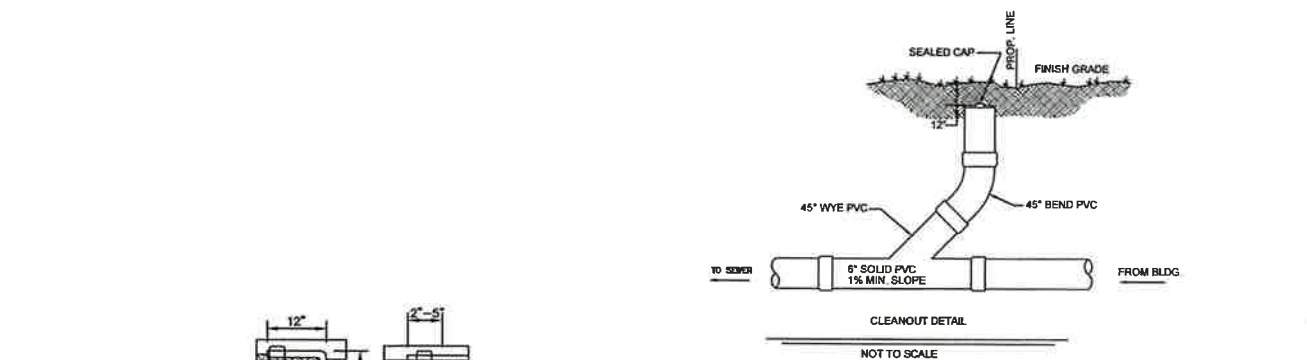
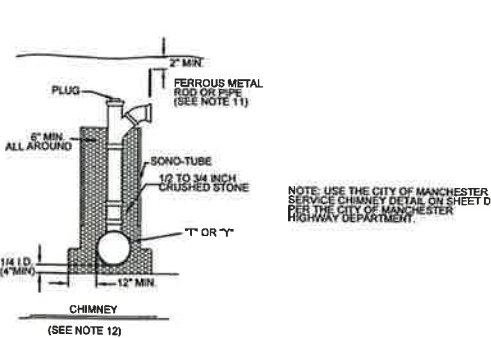
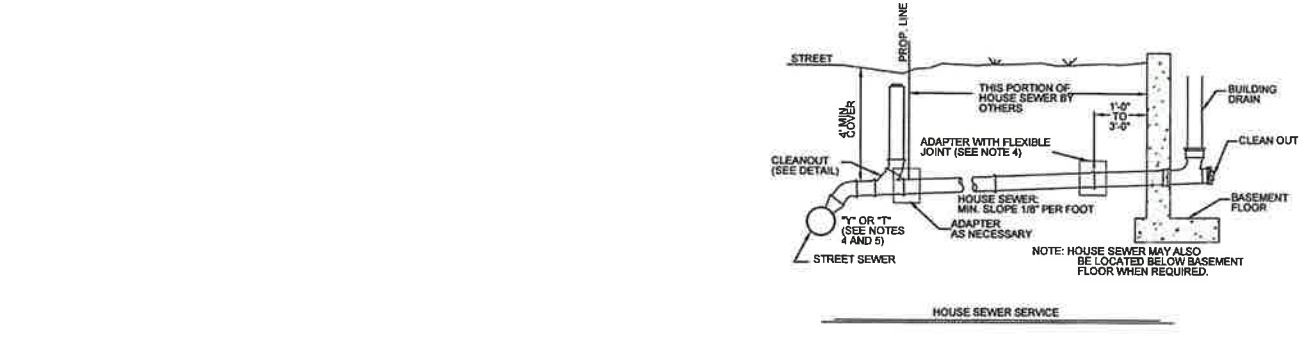
603-772-4746
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Plan Name:	EROSION AND SEDIMENT CONTROL DETAILS
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No

E1

SHEET 45 OF 46
JBE PROJECT NO. 21090



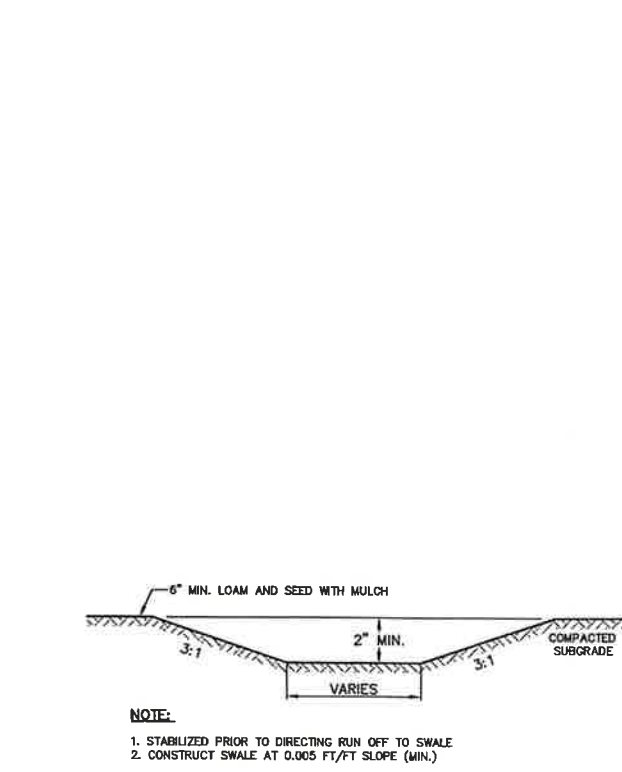
- NOTES:**
1. THERE SHALL BE NO PLASTIC, OR MULTI-FILAMENT OR MONOFILAMENT POLYPROPYLENE NETTING OR MESH WITH AN OPENING SIZE OF GREATER THAN 1/8 INCH MATERIAL UTILIZED.
 2. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED. NOTE: WHEN USING CELL-O-SEED DO NOT SEED PREPARED AREA. CELL-O-SEED MUST BE INSTALLED WITH PAPER SIDE DOWN.
 3. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 6" DEEP BY 6" WIDE TRENCH WITH APPROXIMATELY 12" OF BLANKET EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE BLANKET WITH A ROW OF STAPLES/STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF BLANKET BACK OVER SEED AND COMPACTED SOIL. SECURE BLANKET OVER COMPACTED SOIL WITH A ROW OF STAPLES/STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE BLANKET.
 4. ROLL THE BLANKETS (A) DOWN OR (B) HORIZONTALLY ACROSS THE SLOPE. BLANKETS WILL UNROLL WITH APPROPRIATE SIDE AGAINST THE SOIL SURFACE. ALL BLANKETS MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS SHOWN IN THE STAPLE PATTERN GUIDE. WHEN USING OPTIONAL DOT SYSTEM, STAPLES/STAKES SHOULD BE PLACED THROUGH EACH OF THE COLORED DOTS CORRESPONDING TO THE APPROPRIATE STAPLE PATTERN.
 5. THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH APPROXIMATELY 2"-5" OVERLAP DEPENDING ON BLANKET TYPE. TO ENSURE PROPER SEAM ALIGNMENT, PLACE THE EDGE OF THE OVERLAPPING BLANKET (BLANKET BEING INSTALLED ON TOP) EVEN WITH THE COLORED SEAM STITCH ON THE PREVIOUSLY INSTALLED BLANKET.
 6. CONSECUTIVE BLANKETS SPLICED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATE 3" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART ACROSS ENTIRE BLANKET WIDTH. NOTE: IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 6" MAY BE NECESSARY TO PROPERLY SECURE THE BLANKETS.

NORTH AMERICAN GREEN
 14649 HIGHWAY 41 NORTH
 EVANSVILLE, INDIANA 47725
 1-800-772-2040

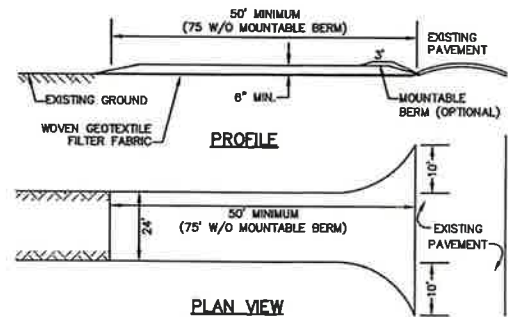
EROSION CONTROL BLANKET SLOPE INSTALLATION
 NORTH AMERICAN GREEN (800) 772-2040

NOT TO SCALE

HOUSE SEWER SERVICE
 NOT TO SCALE

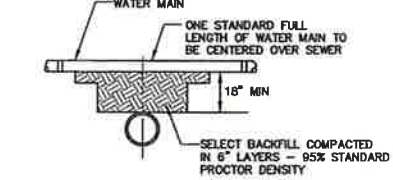


VEGETATED SWALE
 NOT TO SCALE



- NOTES:**
1. STONE FOR STABILIZED CONSTRUCTION ENTRANCE SHALL BE 3 INCH STONE, RECLAIMED STONE, OR RECYCLED CONCRETE EQUIVALENT.
 2. THE LENGTH OF THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 50 FEET, 75' WITHOUT A MOUNTABLE BERM, AND EXCEPT FOR A SINGLE RESIDENTIAL LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY.
 3. THICKNESS OF THE STONE FOR THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 6 INCHES.
 4. THE WIDTH OF THE ENTRANCE SHALL NOT BE LESS THAN THE FULL WIDTH OF THE ENTRANCE WHERE INGRESS OR EGRESS OCCURS, OR 10 FEET, WHICHEVER IS GREATER.
 5. GEOTEXTILE FILTER FABRIC SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING THE STONE. FILTER FABRIC IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENTIAL LOT.
 6. ALL SURFACE WATER THAT IS FLOWING TO OR DIVERTED TOWARD THE CONSTRUCTION ENTRANCE SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A STONE BERM WITH 5:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PIPE.
 7. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO THE PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED, OR TRACKED ONTO THE PUBLIC RIGHT-OF-WAY MUST BE REMOVED PROMPTLY.

STABILIZED CONSTRUCTION ENTRANCE
 NOT TO SCALE



- SEPARATION NOTES:**
1. WATER MAINS SHALL BE LAID AT LEAST 10 FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED SEWERS. THE DISTANCE SHALL BE MEASURED EDGE TO EDGE.
 2. WATER MAINS CROSSING SEWERS SHALL BE LAID TO PROVIDE A MINIMUM VERTICAL DISTANCE OF 18 INCHES BETWEEN PIPES. SEWER PIPE JOINTS SHALL BE LOCATED AT LEAST 6 FEET HORIZONTALLY FROM THE WATER MAIN.

TYPICAL WATER / SEWER SEPARATION
 NOT TO SCALE

- NOTES:**
1. MINIMUM SIZE PIPE FOR HOUSE SERVICE SHALL BE 4 INCHES.
 2. PIPE AND JOINT MATERIALS:
 - A. VITRIFIED CLAY PIPE:
 1. PIPE AND FITTINGS SHALL BE EXTRA STRENGTH CLAY PIPE CONFORMING TO THE REQUIREMENTS OF ASTM C-700.
 2. JOINTS SHALL BE MADE WITH OIL RESISTANT GASKETS IN ACCORDANCE WITH ASTM C-425 TYPE III MANUFACTURERS INSTRUCTIONS FOR INSTALLATION SHALL BE FOLLOWED.
 - B. UP.V.C. (POLY VINYL CHLORIDE) PIPE:
 1. PIPE AND FITTINGS SHALL CONFORM TO THE MOST RECENT REQUIREMENTS OF ASTM SPECIFICATIONS FOR TYPE PSM POLY VINYL CHLORIDE (P.V.C.) SEWER PIPE AND FITTINGS, DESIGNATION D-3034 AND ASTM SPECIFICATIONS FOR SEWER PIPE, JOINTS USING ELASTOMERIC SEALS, DESIGNATION D-3212.
 2. JOINTS SHALL BE OF THE ELASTOMERIC GASKET TYPE. SOLVENT CEMENT JOINTS SHALL NOT BE ALLOWED.
 - C. CAST IRON PIPE FITTINGS AND JOINTS:
 1. CAST IRON PIPE AND FITTINGS SHALL CONFORM TO THE FOLLOWING STANDARDS OF THE AMERICAN NATIONAL STANDARDS INSTITUTE: A21.1 THICKNESS DESIGN OF CAST IRON PIPE A21.4 CEMENT MORTAR LINING FOR CAST IRON PIPE AND FITTINGS A21.8 CAST IRON PIPE CENTRIFUGALLY CAST IN METAL MOLDS FOR WATER OR OTHER LIQUIDS. A21.8 CAST IRON PIPE CENTRIFUGALLY CAST IN SAND LINED MOLDS FOR WATER OR OTHER LIQUIDS. A21.10 CAST IRON FITTINGS, 2 INCHES THROUGH 48 INCHES FOR WATER AND OTHER LIQUIDS.
 2. JOINTS SHALL BE OF THE MECHANICAL OR PUSH ON TYPE JOINTS AND GASKETS SHALL CONFORM TO: A21.11 RUBBER GASKET JOINTS FOR CAST IRON PRESSURE PIPE AND FITTINGS.
 - D. DUCTILE IRON PIPE, FITTINGS AND JOINTS:
 1. DUCTILE IRON PIPE AND FITTINGS SHALL CONFORM TO THE STANDARDS OF THE UNITED STATES OF AMERICA STANDARDS INSTITUTE: A21.50 THICKNESS DESIGN OF DUCTILE IRON PIPE AND WITH ASTM A536 DUCTILE IRON CASTINGS A21.51 DUCTILE IRON PIPE CENTRIFUGALLY CAST IN METAL MOLDS OR SAND LINED MOLDS FOR WATER OR OTHER LIQUIDS.
 2. JOINTS SHALL BE AS SPECIFIED IN C2 ABOVE, CAST IRON PIPE JOINTS.
 3. DAMAGED PIPE SHALL BE REJECTED AND REMOVED FROM THE JOB SITE.
 4. JOINTS SHALL BE DEPENDENT UPON A NEOPRENE OR ELASTOMERIC GASKET FOR WATER TIGHTNESS. ALL JOINTS SHALL BE PROPERLY MATCHED WITH THE PIPE MATERIALS USED. WHERE DIFFERING MATERIALS ARE TO BE CONNECTED, AS AT THE STREET SEWER "Y" OR AT THE FOUNDATION WALL, APPROPRIATE ADAPTERS SHALL BE USED.
 5. U "T" AND "Y" WHERE A "T" OR "Y" IS NOT AVAILABLE IN THE EXISTING STREET SEWER, AN APPROPRIATE CONNECTION SHALL BE MADE IN THE SEWER, FOLLOWING CEMENTED SADDLE TAPPED INTO A SMOOTHLY DRILLED OR SAWN OPENING. THE PRACTICE OF BREAKING AN OPENING WITH A SLEDGE HAMMER, STUFFING CLOTH (OR OTHER SUCH MATERIAL) AROUND THE JOINT, OR APPLYING MORTAR TO HOLD THE CONNECTION AND ANY OTHER SIMILAR CRUDE PRACTICES OR INEPT OR HASTY IMPROVISATIONS WILL NOT BE PERMITTED. THE CONNECTION SHALL BE CONCRETE ENCASED, AS SHOWN IN THE DETAIL, UP TO AND INCLUDING 15" DIAMETER.

6. PIPE INSTALLATION. U THE PIPE SHALL BE HANDLED, PLACED AND JOINTED IN ACCORDANCE WITH INSTALLATION GUIDES OF THE APPROPRIATE MANUFACTURER. IT SHALL BE CAREFULLY BEDDED ON A 4 INCH LAYER OF CRUSHED STONE AND/OR GRAVEL, AS SPECIFIED IN NOTE 10, BEDDING AND RE-FILL FOR A DEPTH OF 12 INCHES ABOVE THE TOP OF THE PIPE SHALL BE CAREFULLY AND THOROUGHLY TAMPED BY HAND OR WITH APPROPRIATE MECHANICAL DEVICES. THE PIPE SHALL BE LAID AT A CONTINUOUS AND CONSTANT GRADE FROM THE STREET SEWER CONNECTION TO THE HOUSE FOUNDATION AT A GRADE OF NOT LESS THAN 1/8 INCH PER FOOT. PIPE JOINTS MUST BE MADE UNDER DRY CONDITIONS. IF WATER IS PRESENT, ALL NECESSARY STEPS SHALL BE TAKEN TO DEWATER THE TRENCH.
7. TESTING: THE COMPLETED HOUSE SEWER SHALL BE SUBJECTED TO A LEAKAGE TEST IN ANY OF THE FOLLOWING MANNERS (PRIOR TO BACKFILLING):
 - A. AN OBSERVATION "T" SHALL BE INSTALLED AS SHOWN. WHEN READY TESTING, AN INFLATABLE BLADDER OR PLUG SHALL BE INSERTED JUST UPSTREAM FROM THE OPENING IN THE "T". AFTER INFLATION, WATER SHALL BE INTRODUCED INTO THE SYSTEM ABOVE THE PLUG TO A HEIGHT OF 5 FEET ABOVE THE LEVEL OF THE PLUG.
 - B. THE PIPE SHALL BE LEFT EXPOSED AND LIBERALLY HOSED WITH WATER TO IMULATE, AS NEARLY AS POSSIBLE, WET TRENCH CONDITIONS. IF THE TRENCH IS WET, THE GROUND WATER SHALL BE PERMITTED TO RISE IN THE TRENCH OVER THE PIPE. INSPECTIONS FOR LEAKS SHALL BE MADE THROUGH THE CLEANOUT WITH A FLASHLIGHT. * DOES NOT APPLY TO INSTALLATIONS WHERE "TS" AND "YS" ARE USED
 - C. DRY FLUORESCENT DYE SHALL BE SPRINKLED INTO THE TRENCH OVER THE PIPE. IF THE TRENCH IS DRY, THE PIPE SHALL BE LIBERALLY HOSED WITH WATER. IF THE TRENCH IS WET, GROUND WATER SHALL BE PERMITTED TO RISE IN THE TRENCH OVER THE PIPE. OBSERVATION FOR LEAKS SHALL BE MADE IN THE FIRST MANHOLE DOWNSTREAM. LEAKAGE OBSERVED IN ANY OF THE ABOVE, ALTERNATE TESTS SHALL BE CAUSE FOR NON-ACCEPTANCE AND THE PIPE SHALL BE DUG UP, IF NECESSARY, AND RELAID SO AS TO ASSURE WATER TIGHTNESS.
8. ILLEGAL CONNECTION: NOTHING BUT SANITARY WASTE FLOW FROM THE HOUSE TOILETS, SINKS, LAUNDRY, ETC. SHALL BE PERMITTED. ROOF LEADERS, FOOTING DRAINS, SUMP PUMPS OR ANY OTHER SIMILAR CONNECTION CARRYING RAIN WATER, DRAINAGE OR GROUND WATER SHALL NOT BE PERMITTED.
9. HOUSE WATER SERVICE SHOULD NOT BE LAID IN THE SAME TRENCH AS THE SEWER SERVICE, BUT WHEN NECESSARY, SHALL BE PLACED ABOVE AND TO ONE SIDE OF THE HOUSE SEWER AS SHOWN.
10. BEDDING: SCREENED GRAVEL AND/OR CRUSHED STONE FREE FROM CLAY, LOAM, ORGANIC MATERIAL AND MEETING ASTM C33-67, 100%-PASSING 1 INCH SCREEN 90-100%-PASSING 3/4 INCH SCREEN 20-55%-PASSING 3/8 INCH SCREEN 0-10%-PASSING #4 SIEVE 0-5%-PASSING #8 SIEVE WHERE ORDERED BY THE ENGINEER TO STABILIZE THE TRENCH BASE, SCREENED GRAVEL OR CRUSHED STONE (1/2 TO 3/4 INCH) SHALL BE USED.
11. LOCATION: THE LOCATION OF THE "T" OR "Y" SHALL BE RECORDED AND FILED IN THE MUNICIPAL RECORDS. IN ADDITION, A FERROUS METAL ROD OR PIPE SHALL BE PLACED OVER THE "T" OR "Y", AS DESCRIBED IN THE TYPICAL "CHIMNEY" DETAIL, TO AID IN LOCATING THE BURIED PIPE WITH A DIP NEEDLE OR PIPE FINDER.
12. CHIMNEYS: IF VERTICAL DROP INTO THE SEWER IS GREATER THAN 4 FEET, A CHIMNEY SHALL BE CONSTRUCTED FOR THE HOUSE CONNECTION.

Design: JAC	Draft: LAZ	Date: 04/28/21
Checked: JAC	Scale: AS NOTED	Project No.: 21090
Drawing Name: 21090-PLAN.dwg		
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10	9/8/22	REVISED PER CITY & NHDES AOT COMMENTS	LAZ
9	8/29/22	REVISED PER NHDES AOT COMMENTS	LAZ
8	8/3/22	REVISED PER NHDES AOT COMMENTS	LAZ
7	7/19/22	REVISED UTILITIES	LAZ
6	8/29/22	REVISED PER TRAFFIC COMMENTS	LAZ
REV.	DATE	REVISION	BY

Designed and Produced in NH

J/B Jones & Beach Engineers, Inc.

65 Portsmouth Ave. PO Box 219 Stratham, NH 03885

Civil Engineering Services

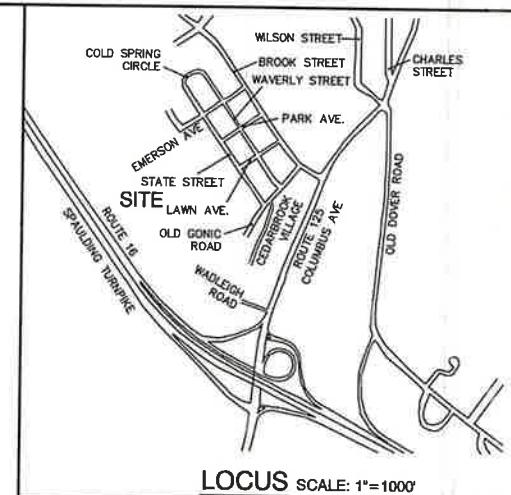
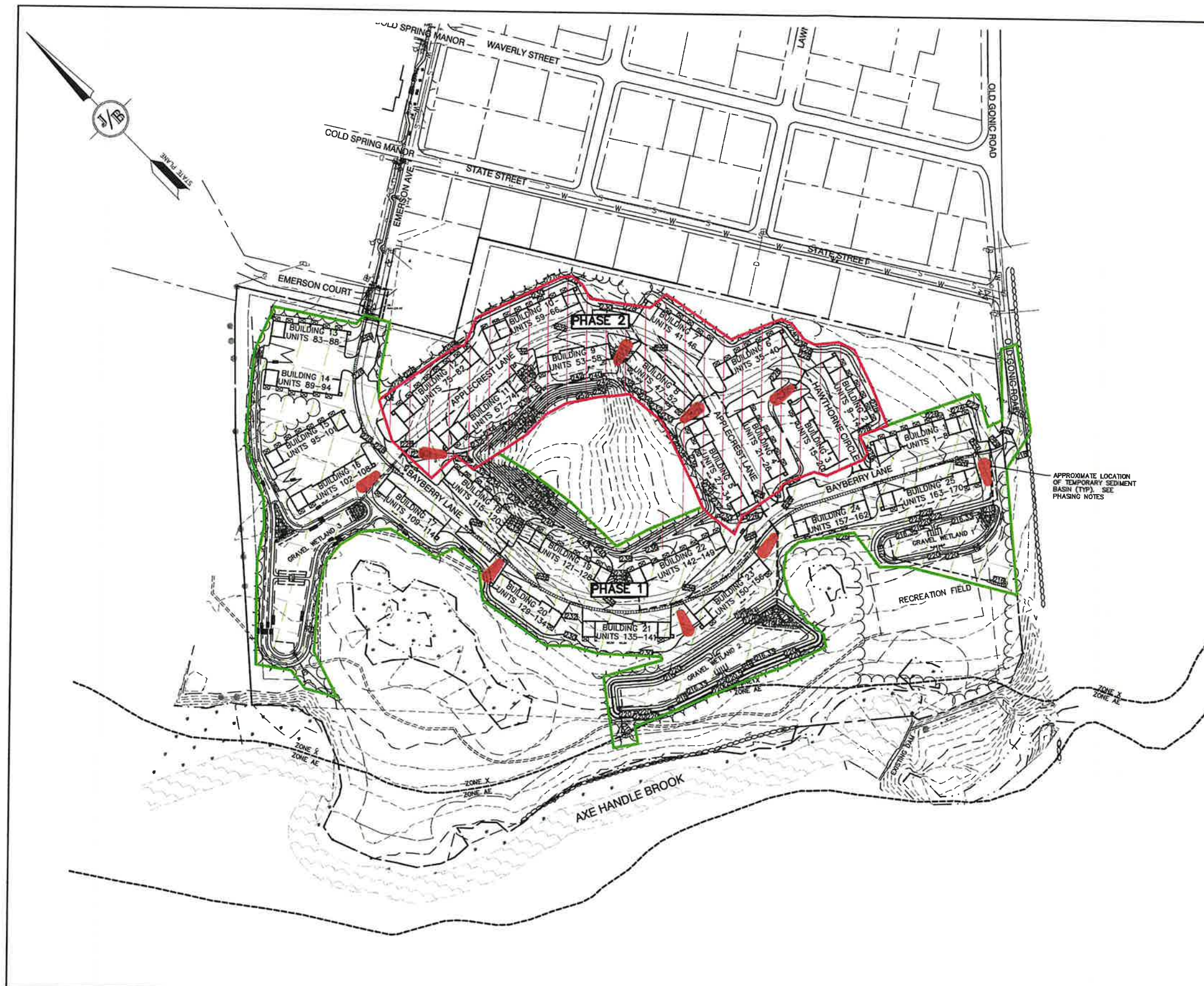
603-772-4746 FAX: 603-772-0227 E-MAIL: JBE@JONESANDBEACH.COM

Plan Name:	DETAIL SHEET
Project:	BAYBERRY COMMONS 19 OLD GONIC ROAD, ROCHESTER, NH
Owner of Record:	LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE 19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148

DRAWING No.

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SHEET 44 OF 46
JBE PROJECT NO. 21090

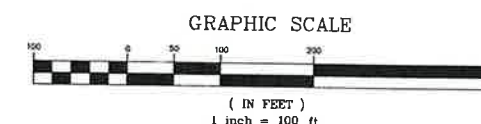


PHASING NOTES:

- PHASING IS MANDATORY PER NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICES AND NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICES ALTERATION OF TERRAIN SHALL BE NOTIFIED OF ANY ALTERATION OF THE PHASING PLAN.
- TEMPORARY SEDIMENT BASINS SHALL BE CONSTRUCTED PRIOR TO THE ROUGH GRADING OF EACH PHASE.
- EACH PHASE MUST BE STABILIZED BEFORE PROCEEDING TO SUCCESSIVE PHASES.
- AN ENVIRONMENTAL MONITOR SHALL BE EMPLOYED TO EVALUATE THE SITE DURING CONSTRUCTION.
- AREAS MUST BE STABILIZED PRIOR TO WINTER CONSTRUCTION SEASON FROM OCTOBER 15 THROUGH MAY 15 PURSUANT TO Env-Wq 1505.06 (1 AC OPEN) UNLESS A WINTER CONSTRUCTION/STABILIZATION PLAN IS SUBMITTED AND APPROVED.

PHASING SCHEDULE:

PHASE 1	10.9 AC
CONSTRUCT:	BAYBERRY LANE ALL BUILDINGS FRONTING ON BAYBERRY LANE ALL PONDS
PHASE 2	5.4 AC
CONSTRUCT:	APPLECREST LANE HAWTHORNE CIRCLE ALL BUILDINGS FRONTING ON APPLECREST LANE AND HAWTHORNE CIRCLE



PROJECT PARCEL
CITY OF ROCHESTER
TAX MAP 131, LOT 10

APPLICANT
GREEN AND COMPANY
11 LAFAYETTE ROAD
NORTH HAMPTON, NH 03862

TOTAL LOT AREA
1,309,695 SQ. FT. ±
30.07 ACRES ±

Design: JAC Draft: LAZ Date: 04/29/21
Checked: JAC Scale: AS NOTED Project No.: 21090
Drawing Name: 21090-PLAN.dwg

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REV.	DATE	REVISION	BY

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Plan Name: **PHASING PLAN**
Project: **BAYBERRY COMMONS
19 OLD GONIC ROAD, ROCHESTER, NH**
Owner of Record: **LEO P. LACOUTURE REV. TRUST & WILLIAM B. LACOUTURE
19 OLD GONIC RD., ROCHESTER, NH 03867 BK 4093 PG 148**

DRAWING No.

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SHEET 46 OF 46
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